

TOWN OF  
CLIFTON PARK  
ADA TRANSITION  
PLAN

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# 1.0 INTRODUCTION

## 1.1 Overview

The purpose of this ADA Self-Analysis and Transition Plan is to ensure that the Town of Clifton Park pedestrian infrastructure in the public right-of-way is accessible for everyone, including people with disabilities. Federal standards for pedestrian facilities in the public right of way proposed in 2011 were finalized and became effective in September 2023, were adopted by the US Department of Transportation in December 2024, and will be fully enforceable once adopted by the US Department of Justice. The Town of Clifton Park has made a significant and long-term commitment to improving the accessibility of pedestrian facilities in the public-right-of-way.

Clifton Park has identified and evaluated all pedestrian infrastructure in the public right-of-way including curb ramps, sidewalks and multi-use paths, crosswalks, pedestrian signals, and pedestrian accessible transit stops.

This document was funded by the Capital Region Transportation Council (Transportation Council), in part through a grant from the Federal Highway Administration, U.S. Department of Transportation. It is limited to pedestrian transportation facilities in the public right of way as well as shared use paths with a transportation function, and does not include any other programs, services, or activities such as buildings, parks, playgrounds, recreational trails, websites, or meeting locations. The views and opinions expressed herein do not necessarily state or reflect those of the U. S. Department of Transportation.

## 1.2 Mandate – Americans with Disabilities Act

The federal legislation, known as the Americans with Disabilities Act (ADA), enacted on July 26, 1990, provides comprehensive civil rights protections to persons with disabilities in the areas of employment, state and local government services, and access to public accommodations, transportation, and telecommunications.

## 1.3 Transition Plan

Title II of the ADA specifically applies to “public entities” (state and local governments) and the programs, services, and activities they deliver. Title II regulations require that each public agency develop a Transition Plan (also referred to as “Plan”) to bring any non-compliant programs, services, and activities into compliance.

The Plan shall, at a minimum:

- Identify physical obstacles in the public entity's facilities that limit the accessibility of its programs or activities to individuals with disabilities;
- Describe in detail the methods that will be used to make the facilities accessible;

- Specify the schedule for taking the steps necessary to achieve compliance with Title II and, if the time period of the transition plan is longer than one year, identify steps that will be taken during each year of the transition period;
- Indicate the official responsible for implementation of the plan.

Transition plans provide a method for a public entity to schedule and implement ADA required improvements to existing facilities. Before a transition plan can be developed, an inventory of the existing facilities must be developed.

Because the ADA is a civil rights law, the federal enforcement entity for the ADA is the Civil Rights Division of the US Department of Justice (DOJ). The DOJ enforces ADA through various means: establishing standards for meeting the requirements of the ADA; coordinating enforcement activities of other federal agencies; and, as necessary, taking legal action to require compliance with ADA by state and local governments.

The Access Board is a federal agency that promotes equality for people with disabilities through leadership in accessible design and the development of accessibility guidelines and standards. As a coordinating body, the scope of their recommendations and guidance includes but is not limited to public rights of way, public buildings, and medical facilities.

## 1.4 Public Rights of Way Accessibility Guidelines (PROWAG)

The United State Access Board issued its final rule for accessibility guidelines for pedestrian facilities in public rights-of-way in September 2023. Not yet formally adopted by the US Department of Justice, the Public Rights of Way Accessibility Guidelines (PROWAG) were adopted by the US Department of Transportation in December 2024. They were utilized in evaluating pedestrian facilities in the preparation of this plan.

The PROWAG provide design guidance for a wide variety of facilities in the public right of way, including curb ramps, sidewalks, street crossings, transit stops, and signage. Many of the guidelines are particularly sensitive to people who use a wheelchair or have low vision, though the guidance is intended to eliminate all access barriers for people who have a disability.

## 1.5 Public Right of Way (ROW) and Jurisdiction

Defining jurisdictional boundaries or ownership of facilities within the public ROW and identifying the responsible entity is central to the successful implementation of the ADA Transition Plan. Three jurisdictional entities were identified in preparing this plan including the Town of Clifton Park, Saratoga County, and the State of New York.

Although three jurisdictional entities possess rights-of-way within the Town of Clifton Park, the legal responsibility of maintaining and improving non-conforming or deficient facilities does not necessarily fall to the owner of the right-of-way.

According to New York State Highway Law (Chapter 25, Article 7, §140), the State of New York may install sidewalks. However, notwithstanding an agreement to the contrary, once installed, the Town of Clifton Park is obligated to maintain those sidewalks. Similarly, the Town of Clifton Park must maintain sidewalks along county roadways (Chapter 25, Article 7, § 140) unless there is an agreement to the contrary.

The following table (Table 1) identifies the entity responsible for the maintenance and improvement of pedestrian facilities within the Town of Clifton Park. This table is representative only of jurisdictional entities at the Town of Clifton Park, County, or State level.

**TABLE 1: RESPONSIBLE ENTITY FOR MAINTENANCE OF PEDESTRIAN FACILITIES**

<b>FACILITY TYPE</b>	Town of Clifton Park Right of Way	County Right of Way	New York State Right of Way
<b>SIDEWALKS</b>	Town of Clifton Park	Town of Clifton Park	Town of Clifton Park
<b>CURB RAMPS</b>	Town of Clifton Park	Town of Clifton Park	Town of Clifton Park
<b>CROSSWALKS</b>	Town of Clifton Park	Saratoga County	New York State
<b>PEDESTRIAN CROSSING SIGNALS</b>	Town of Clifton Park	Saratoga County	New York State

## 1.6 Installation of Conforming Facilities During Roadway Alteration Projects

In accordance with the *Department of Justice(DOJ)/Department of Transportation (DOT) Joint Technical Assistance on the Title II of Americans with Disabilities Act Requirements to Provide Curb Ramps when Streets, Roads, or Highways are Altered through Resurfacing* (<https://archive.ada.gov/doj-fhwa-ta.htm>) – when pedestrian walkways intersect a curb, and the adjacent roadway is being altered including by resurfacing, the entity facilitating the roadway alteration is also responsible for the provision of an accessible curb ramp. The same principle exists if an entity is undertaking a roadway alteration project that requires the removal of other pedestrian facilities, such as sidewalks. If the facility is removed, it must be reinstalled and conform to the most current accessibility standards. The DOJ document referenced above provides guidance on the definition of a roadway alteration to include: “reconstruction, rehabilitation, *resurfacing*, widening, and projects of similar scale and effect.”

## 1.7 Identified Obstacles in the Public Right-of-Way

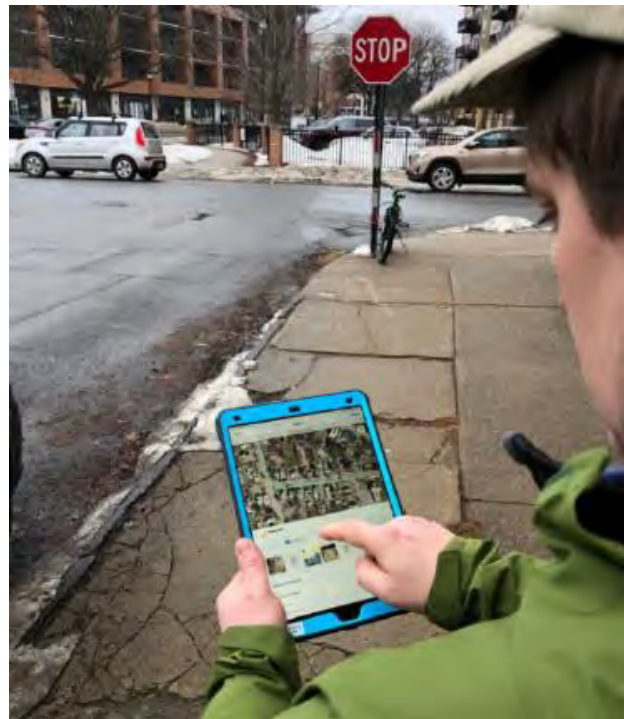
The Town of Clifton Park has conducted a detailed evaluation of obstacles to pedestrian infrastructure along all public rights-of-way within the Town, using the methodology and criteria outlined in Appendix A. Sidewalks were evaluated for continuity, missing panels, heaving panels, physical obstructions, and minimum width. Curb ramps, crosswalks, transit stops, and pedestrian signals were evaluated against key PROWAG criteria. The results of the evaluation are presented in the following section(s).

# 2.0 DETAILED EVALUATION

## 2.1 Inventory

An inventory and assessment of all pedestrian facilities located in the Town of Clifton Park on local, county, and state roads was completed by consultants provided by the Capital Region Transportation Council.

Initially, a sidewalk inventory (including related pedestrian features and transit stops) was provided by the Transportation Council and this information was updated based on a review of the most current aerial photography and *Google Street View*. This information was used as a base map for additional data collection and condition assessment. New sidewalks may have been constructed during or shortly before the completion of this document. Over time, changes to the pedestrian infrastructure should be incorporated into the inventory and monitored for continued compliance.



Field data collection was accomplished on a mobile device (a Tablet, iPad, Trimble, or mobile phone) and the ESRI applications *ArcGIS Field Maps and Survey 123*. The consultant's staff were trained in the evaluation and scoring of pedestrian features for compliance and subsequently completed the inventory and collected photographs and other important site-specific information.

Pedestrian features evaluated included sidewalks, transit stops, and key intersection features including curb ramps, pedestrian signals, and crosswalks. Evaluation criteria focused on compliance with key elements adapted from the PROWAG. The evaluation methods are included in Appendix A. To complete the detailed evaluation, an accessibility

condition rating for each of the identified facilities was determined. The Town of Clifton Park used the following ratings and definitions to assess the condition of each sidewalk facility on local, county, and state rights-of-way.

**TABLE 2: CHARACTERISTICS OF ACCESSIBILITY CONDITION FOR SIDEWALKS**

	<b>CHARACTERISTICS</b>	<b>EXAMPLE</b>
<b>1. FULLY ACCESSIBLE</b>	<ul style="list-style-type: none"> <li>Designed to Meet Current Standards</li> </ul>	<ul style="list-style-type: none"> <li>Newly Installed Facility</li> </ul>
<b>2. MOSTLY ACCESSIBLE</b>	<ul style="list-style-type: none"> <li>Provides Access</li> <li>Not Fully Compliant</li> </ul>	<ul style="list-style-type: none"> <li>Curb Ramps <i>Without</i> Detectable Warnings</li> <li>Limited Minor Defects</li> </ul>
<b>3. PARTIALLY ACCESSIBLE</b>	<ul style="list-style-type: none"> <li>Limited Accessibility</li> </ul>	<ul style="list-style-type: none"> <li>Several Minor Defects</li> </ul>
<b>4. NOT ACCESSIBLE</b>	<ul style="list-style-type: none"> <li>Significant Discontinuity</li> <li>Inaccessible</li> </ul>	<ul style="list-style-type: none"> <li>No Curb Ramps</li> <li>Steps</li> <li>Significant Heaving</li> </ul>

Note: Additional information on evaluation criteria, including example imagery of sidewalk conditions, can be found in Appendix A.

To evaluate the accessibility of more complex multi-part features like curb ramps, crosswalks, and transit stops a rating matrix was used. Depending on the severity and number of deficiencies at any one of these features, they were assigned a pass/fail rating.

## 2.2 Sidewalks

The Town of Clifton Park has a total of 32.5 miles of sidewalks and paved trails that have been assessed for compliance with PROWAG. Sidewalks are assessed for missing panels, heaving panels, obstructions, and panel sidewalk width. Sidewalks are rated at the segment level. Typically, one sidewalk segment equates to one block unless external circumstances – such as a driveway or a significant commercial activity like a grocery store – cause a significant break in the sidewalk on that block. Approximately 63 percent of segments in the Town were rated Fully Accessible, no segments were rated not accessible, 35.3 percent were considered Mostly Accessible, and 1.7 percent were identified as partially accessible. In total, 1.7 percent have a rating that requires repair or improvement (Partially Accessible or Not Accessible).

It should be noted that because field evaluation ratings are conducted at the segment level, short segments are less likely to have conditions that would impact their rating, while the opposite is true for long blocks. Knowing this, the Town should use this evaluation as a tool

to identify segments that have barriers that must be remedied and focus remediation efforts on specific barriers rather than entire block segments.

**TABLE 3: BLOCK LEVEL SIDEWALK AND PAVED TRAIL SCORE RESULTS**

<b>ACCESSIBILITY RATING</b>	<b>PERCENTAGE (%) OF TOTAL MILES</b>	<b>LENGTH (MILES)</b>	<b>NUMBER OF BLOCKS</b>	<b>PERCENTAGE (%) OF SEGMENTS</b>
<b>FULLY ACCESSIBLE</b>	61.7%	16.6	276	73.0%
<b>MOSTLY ACCESSIBLE</b>	36.3%	9.8	94	24.9
<b>PARTIALLY ACCESSIBLE</b>	2.0%	0.5	8	2.1%
<b>NOT ACCESSIBLE</b>	0%	0	0	0%
<b>GRAND TOTAL</b>	100%	26.9	378	100%

The most common conditions that create barriers to accessibility in Clifton Park are heaving panels and obstructions, which were present on about two percent of all segments assessed. On paved trails, there were four instances of obstructions, and three instances of pathways that are too narrow. The presence of heaving panels may not result in a Not Accessible or Partially Accessible rating depending on the severity and frequency of heaving. Sidewalk segments rated Mostly Accessible should be closely monitored to ensure these segments remain accessible and conditions do not worsen over time.

A detailed table depicting the Town’s inventory of sidewalks is included in Appendix D.

**TABLE 4A: BLOCK LEVEL SIDEWALK BARRIER CHARACTERISTICS**

<b>BARRIER CHARACTERISTIC</b>	<b>NUMBER OF BLOCKS WITH BARRIER</b>	<b>PERCENT (%) OF ALL BLOCKS</b>
Missing Panel	4	1.5%
Heaving Panel	6	2.3%
Obstruction	6	2.3%
Too Narrow	3	1.2%

**TABLE 4B: BLOCK LEVEL PAVED TRAIL BARRIER CHARACTERISTICS**

<b>BARRIER CHARACTERISTIC</b>	<b>NUMBER OF BLOCKS WITH BARRIER</b>	<b>PERCENT (%) OF ALL BLOCKS</b>
Missing Panel	N/A	N/A
Heaving Panel	N/A	N/A
Obstruction	4	2.0%
Too Narrow	3	1.5%

## 2.3 Curb Ramps

All intersections where sidewalks intersect roadways were observed for the presence of curb ramps, whether the curb ramp was flush with the roadway and whether a Detectable Warning Strip (DWS) was present. 543 intersection access points were evaluated to assess their condition and accessibility.

About 60 percent of the observed curb ramps have a condition that may impact their accessibility, the most frequent of which are related to Detectable Warning Strips. DWS are used to indicate to the visually impaired that they have reached an intersection with a roadway. The absence of DWS was a contributing factor to accessibility in 54 percent of all curb ramps. The most common conditions identified with DWS that may impact accessibility are deterioration and damage of the DWS, followed by insufficient contrast of the DWS with the sidewalk. Additionally, 0.7 percent of curb ramps are not flush with the asphalt where they meet the road, which can create problems for people with mobility issues.

The PROWAG Guidelines specify that DWS shall contrast visually with adjacent walking surfaces, either light-on-dark or dark-on-light.

**TABLE 5: CURB RAMP CONDITIONS**

<b>TYPE OF BARRIER</b>	<b>COUNT</b>	<b>PERCENTAGE</b>
No Curb Ramp Present	10	1.4%
Curb Ramps without Barriers	213	40.1% (of those with curb ramps)
Curb Ramps with Barriers*	317	59.9% (of those with curb ramps)
DWS with Barriers*	31	12.0% (of those with DWS)

\*Curb ramps and DWS strips may have multiple barriers

**TABLE 6: CURB RAMP BARRIERS**

<b>CURB RAMP BARRIERS</b>	<b>COUNT</b>	<b>PERCENTAGE OF THOSE WITH CURB RAMPS</b>
Not Flush with Asphalt	4	0.7%

Inadequate Width	0	0.0%
DWS Absent	272	53.4
DWS with Barriers*	31	12.0% (of those with DWS)

\*Curb ramps and DWS strips may have multiple barriers

**TABLE 7: DWS BARRIERS**

<b>DETECTABLE WARNING STRIP BARRIERS</b>	<b>COUNT</b>	<b>PERCENTAGE OF THOSE WITH DWS</b>
Insufficient Width	1	0.4%
Insufficient Depth	1	0.4%
Insufficient Contrast	8	3.3%
Deterioration/Damage	29	11.9%

In addition to conditions that may create barriers to accessibility, curb ramps and DWS were inventoried for characteristics that may assist the Town in determining repair costs, replacement methods, and standard practices. Most of the Town’s curb ramps serve one direction of travel and most of the Town’s DWS are truncated domes.

**TABLE 8: CURB RAMP INVENTORY CHARACTERISTICS**

<b>CURB RAMP CHARACTERISTIC</b>	<b>COUNT</b>	<b>PERCENTAGE OF CURB RAMPS</b>
Ramp Serves One Directions of Travel	234	96.3%
Ramp Serves Two Directions of Travel	9	3.7%

**TABLE 9: DWS INVENTORY CHARACTERISTICS**

<b>DWS INVENTORY CHARACTERISTICS</b>	<b>COUNT</b>	<b>PERCENTAGE OF DWS</b>
DWS Truncated Domes	237	97.5%
DWS Pavement Grooves	6	2.5%

## 2.4 Pedestrian Signals

Pedestrian signals let motorists know that pedestrians have the right of way, and let pedestrians know when it is their turn to cross a roadway. These signals were evaluated for the presence of a push button to request the crossing phase and/or crossing information, the crossing signal method (audio, visual, or both), and wheelchair accessibility (height and distance of the push button from the access route). The most common condition that may

create barriers for pedestrian signal accessibility is that many signals have only audio or visual cues indicating safe time to cross. 31.2 percent of the Town’s pedestrian signals are visual or audible only, with visual only signals being the most common type.

An accessible pedestrian signal (APS) is defined in PROWAG as a device that communicates information about pedestrian signal timing in non-visual formats such as audible tones or speech messages, and vibrating surfaces. It is recommended that the Town adopt an APS policy to guide implementation of new APS as well as upgrades of existing traffic signals with or without pedestrian signals to include APS installation.

**TABLE 10 PEDESTRIAN SIGNAL CONDITIONS**

Type of Barrier	Count	Percentage of Pedestrian Signals
Both Visual and Audible	55	68.8%
Visual or Audible Only	25	31.2
Visual Only	24	30%
Audible Only	1	1.2%
Not Wheelchair Accessible	1	1.2%

## 2.5 Pedestrian Crossings

Where sidewalks exist, intersections were observed for the presence of a pedestrian access route across the roadway, the condition of the access route, and the condition of marked crossings. All roads and intersections were observed for the presence of a pedestrian access route at both ends of the crossing, the condition of the crossing, whether there is a painted crosswalk, and the condition of the crosswalk paint. The condition and accessibility of 234 crossings was collected. About 98 percent have a complete access route. Fair or poor route condition was a barrier for crossing one complete access route. It was caused by potholes or cracks. Additionally, poor condition of crossing markings was identified at 8.6 percent of all marked crossings. While crosswalk markings are not required under PROWAG, the Manual on Uniform Traffic Control Devices (MUTCD) requires solid white lines on crossings that are marked. Marked crossings that contrast with the roadway may also provide a visual indicator of the pedestrian right of way for the visually impaired.

**TABLE 11: PEDESTRIAN CROSSING CONDITIONS**

TYPE OF BARRIER	COUNT	PERCENTAGE
Incomplete Access Route	3	1.3%
Fair or Poor Route Condition	1	0.43%(of those with complete access

		routes)
Cracks/potholes	1	100%(of those with poor route condition)
Improper Cross-slope	0	0%(of those with poor route condition)
Other	0	0%(of those with poor route condition)

Note: Curb ramps and crosswalks are evaluated separately. There is no relationship between the number of crossings and curb ramps evaluated due to variability of curb ramp presence at crossings.

**TABLE 12: CROSSWALK CONDITIONS**

CONDITION	COUNT	PERCENTAGE
Fair or Poor Crosswalk Condition	12	8.6% (of those with marked crossings)

## 2.6 Transit Stops

The Capital District Transportation Authority (CDTA) provides transit service to the Town. Two transit stops were assessed for accessibility. Transit stops within 50ft of sidewalks were assessed based on their connectivity to existing sidewalks and the conditions of their boarding and alighting area. A boarding and alighting area is defined in PROWAG as a level and stable surface for boarding vehicles. Both transit stops in Clifton Park are serving park and ride lots, and while both transit stops are considered accessible, neither is connected to a sidewalk network.

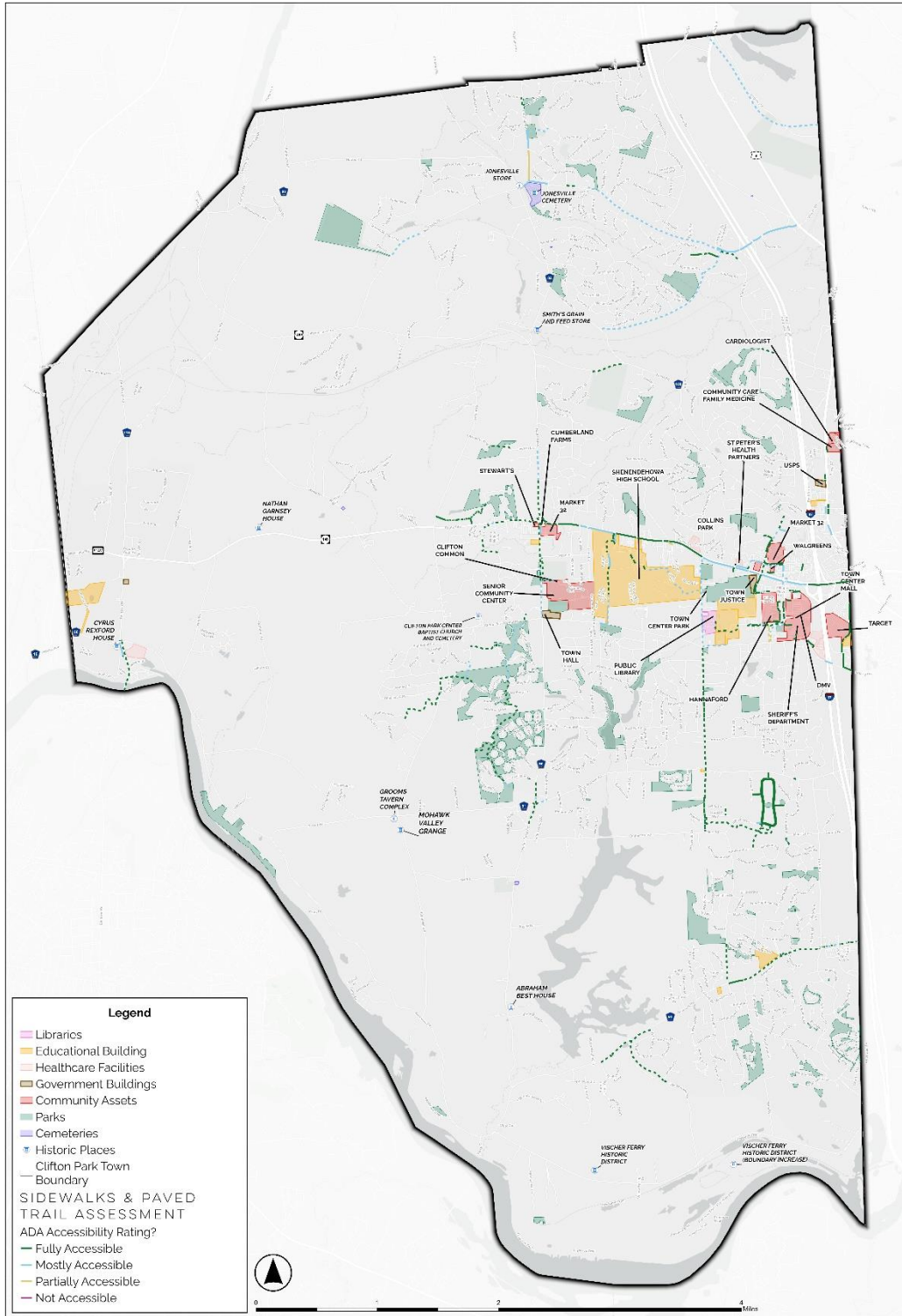
**TABLE 13: TRANSIT STOP CONDITIONS**

Type of Barrier	Count	Percentage of all Stops
Not Connected to Sidewalk Network	2	100%
Boarding and Alighting Areas with Barrier*	0	0%
Insufficient Width	0	0%
Insufficient Depth	0	0%
Non-Accessible Surface Material**	0	0%
Obstructed Access Route	0	0%

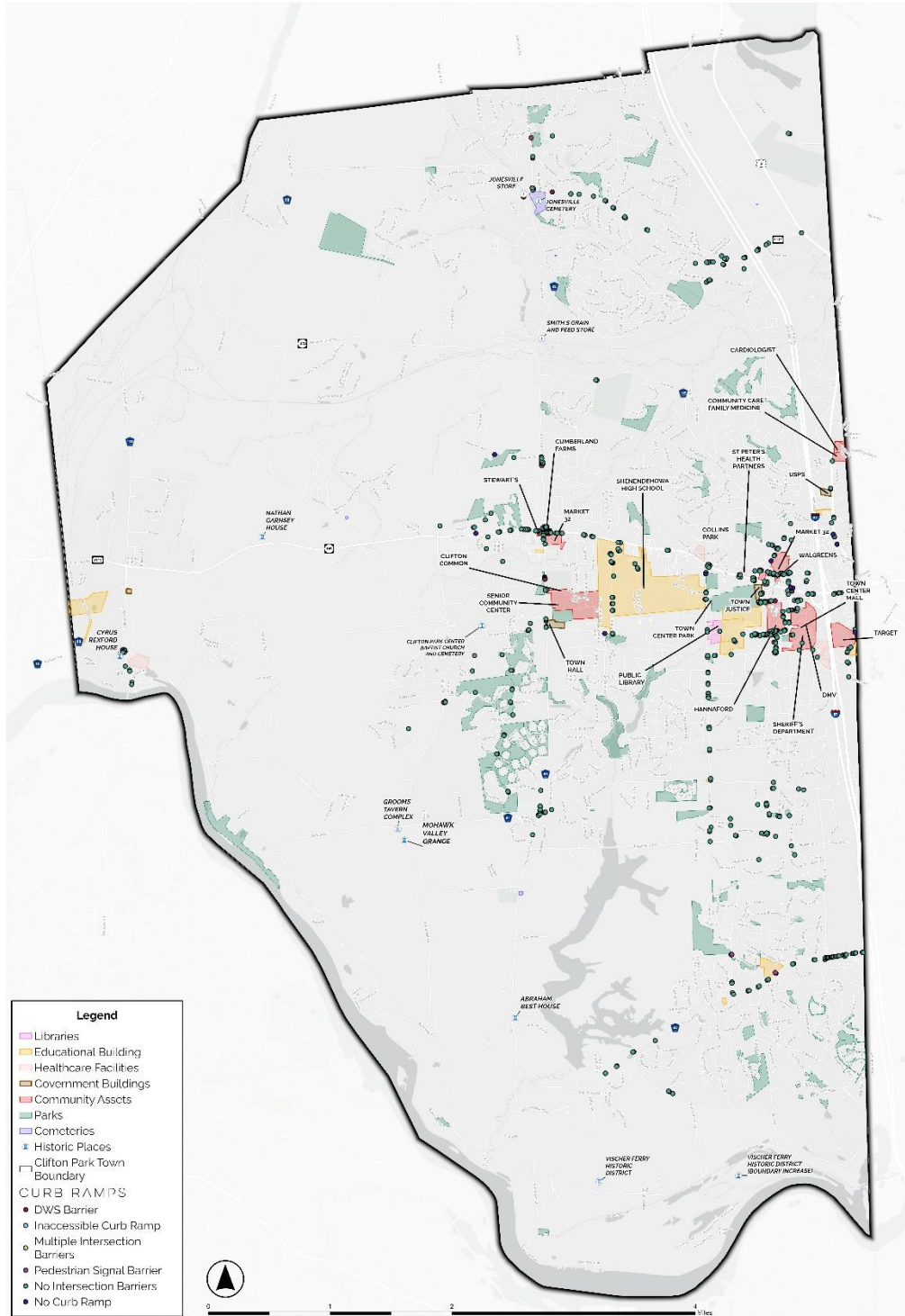
\*Note: Boarding and Alighting Areas may have multiple barriers identified

\*\*Note: Grass or dirt surface

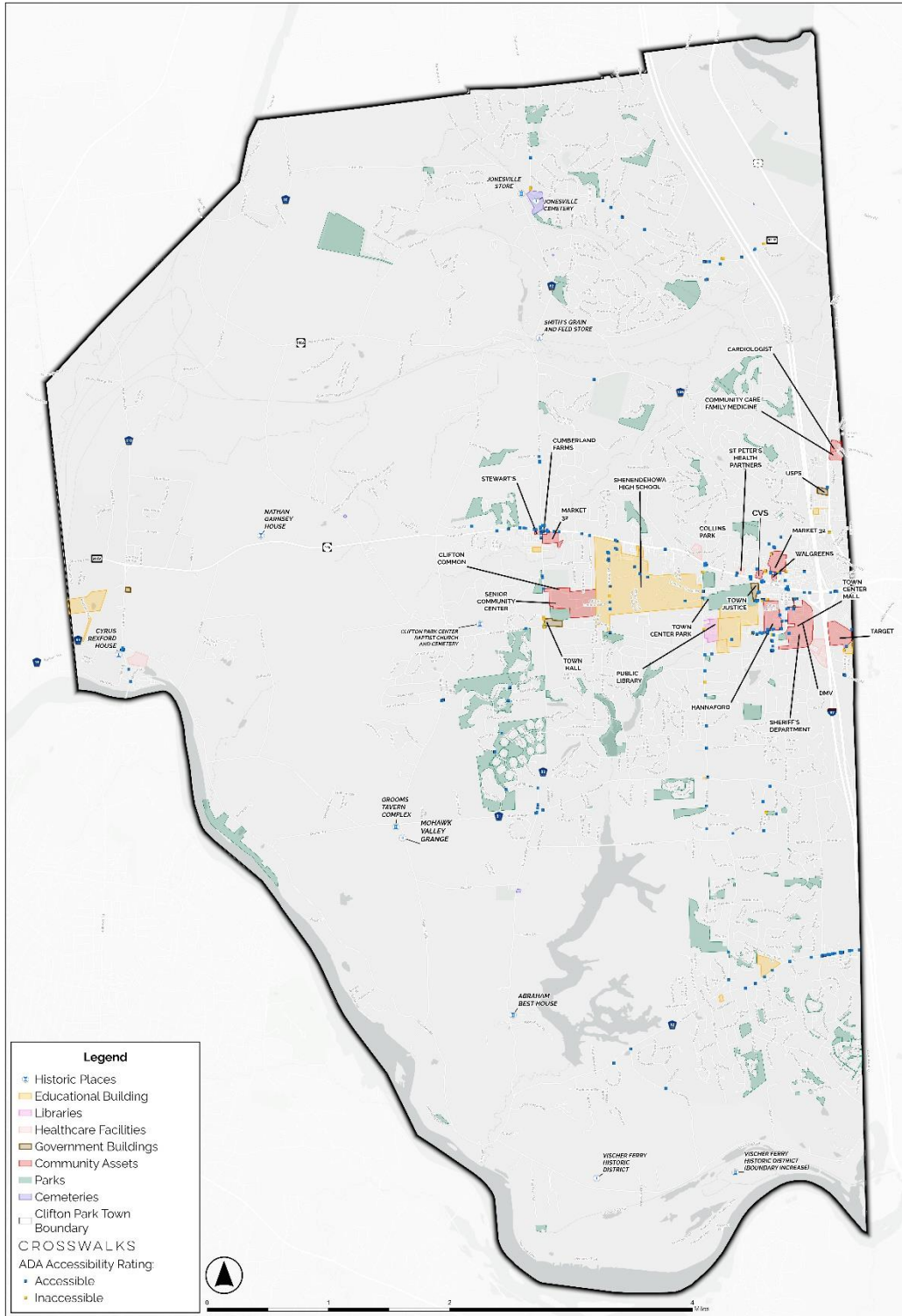
**FIGURE 1: SIDEWALK ADA ASSESSMENT**



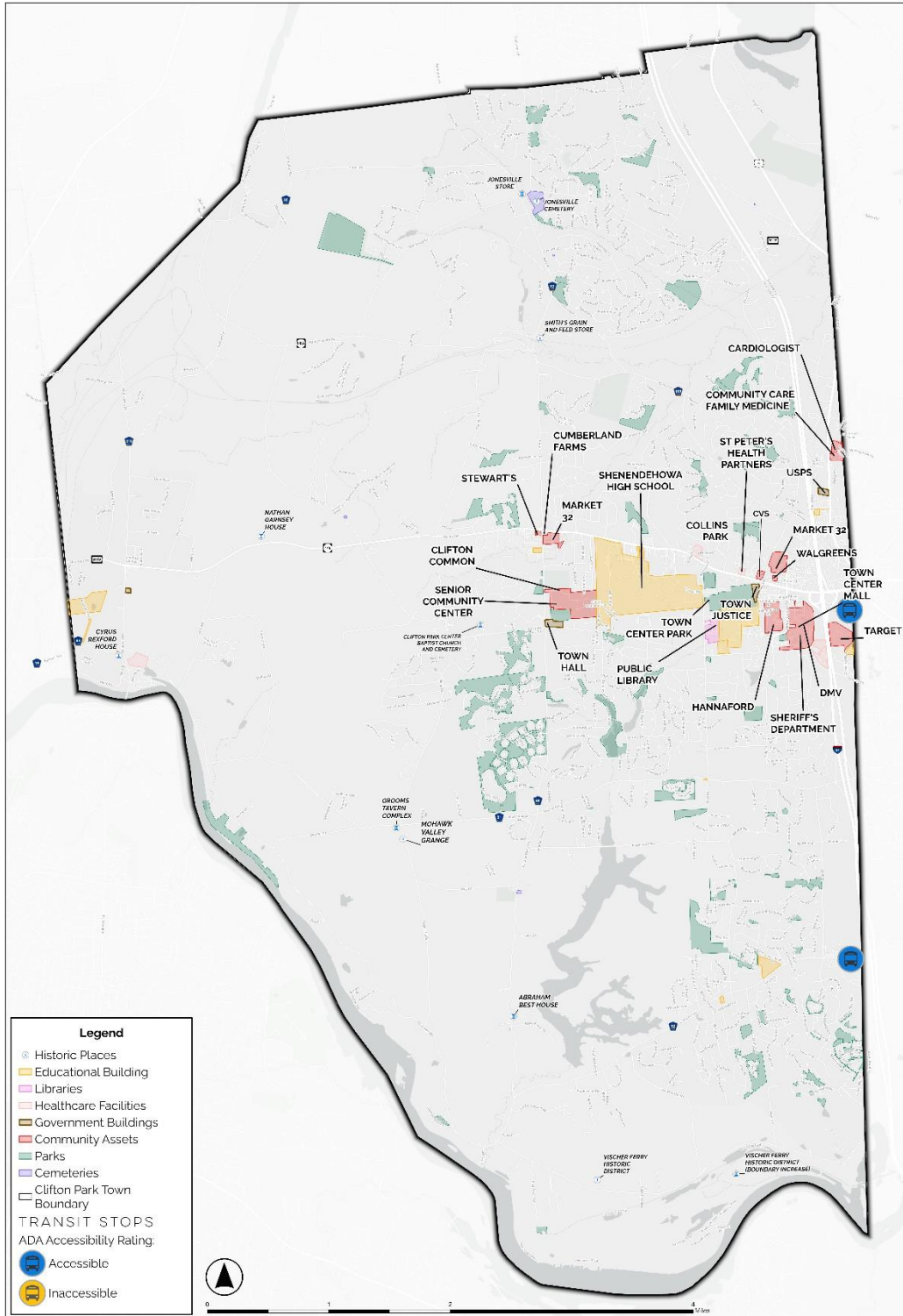
# FIGURE 2: CURB RAMP ADA ASSESSMENT



**FIGURE 3: ADA CROSSING ASSESSMENT**



**FIGURE 4: ADA TRANSIT STOP ASSESSMENT**



# 3.0 METHODS TO REMOVING BARRIERS – POLICIES AND PRIORITIES

The Town of Clifton Park plans to utilize multiple approaches to removing barriers in the public right-of-way, including responding to public complaints, pursuing funding to complete municipal projects, and ensuring the appropriate design and build-out of new construction following the most recent design guidelines. The Town plans to base barrier removal on the location of and accessibility of each facility. Additionally, the Town may modify this schedule to respond to public complaints and to take advantage of public and private improvements.

## 3.1 Location Priority

According to the ADA Title II regulations, the transition plan shall give “priority to walkways serving entities covered by the Act, including State and local government offices and facilities, transportation, places of public accommodation, and employers, followed by walkways serving other areas” (§ 35.150 Existing Facilities (d)(2)). The Town of Clifton Park will prioritize government and community service facilities, the Town Center and major commercial centers, schools and senior centers, and residential areas. In addition to the outlined list below, when initiating repairs, the Town plans to pay particular attention in prioritizing areas in the Town Center area where residents spend much of their time, as well as paying consideration to areas where people are more likely rely on other means of transportation than car ownership. Generally, the Town intends to prioritize areas that serve the most critical functions and needs of residents.

Following Title II regulations, and specific considerations of the Town, the Town of Clifton Park identified its location priority as follows, beginning with the highest priority:

**Priority 1: Government, Community Service and Health Facilities**

**Priority 2: Town Center and Major Commercial Center**

**Priority 3: Schools and Senior Center**

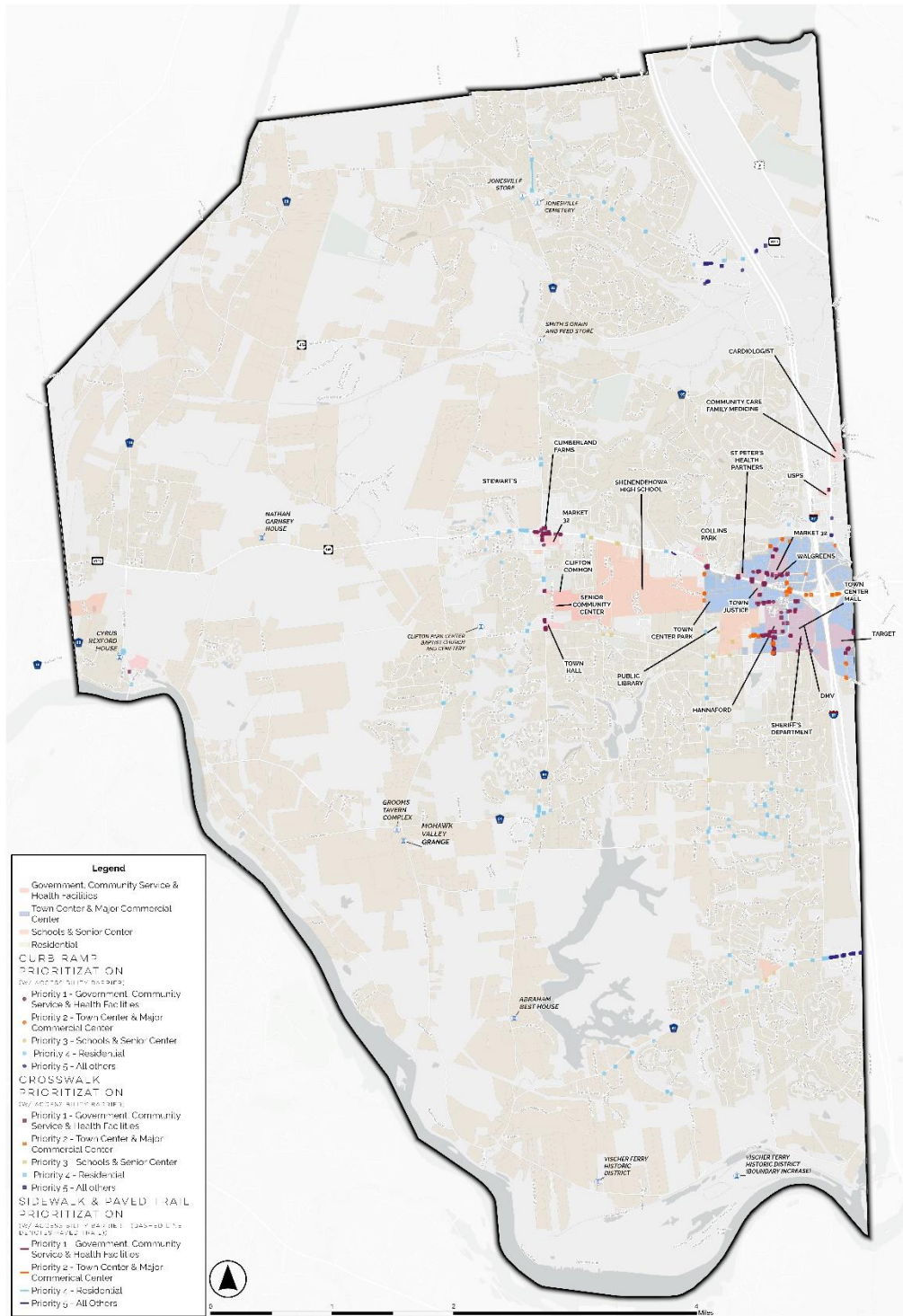
**Priority 4: Residential Areas**

**Priority 5: All Other**

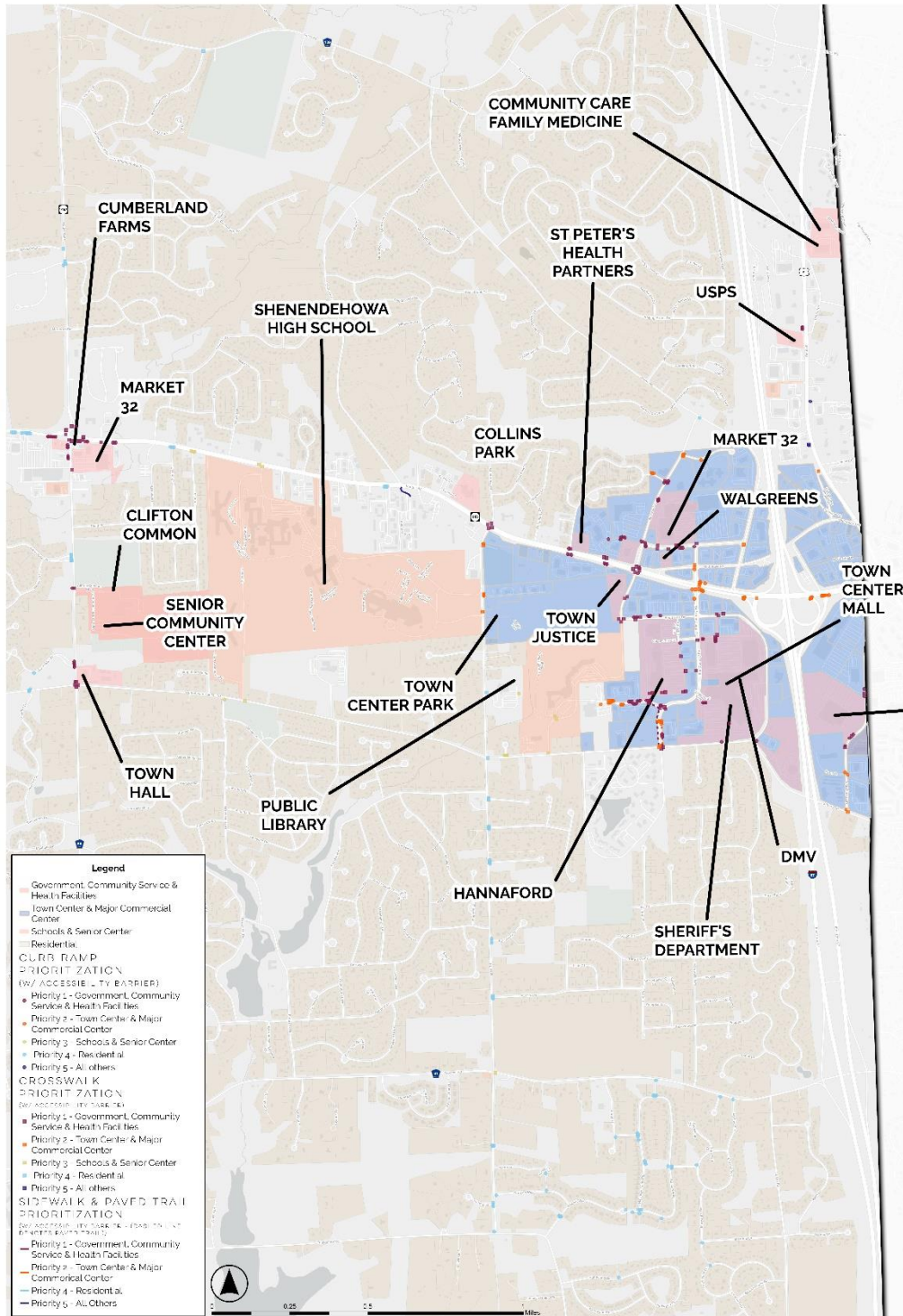
Figures 5 and 6 illustrate the priority sidewalk segments regardless of jurisdiction. Full-size figures are presented in Appendix B.

Figure 5 reflects the prioritization method only for facilities identified through data analysis as “Not Accessible” or “Partially Inaccessible.” Alternatively, Figure 6 identifies all existing sidewalk facilities by the prioritization method, regardless of their current accessibility.

# FIGURE 5A: ADA PRIORITIZATION MAP BY EXISTING CONDITION (TOWN-WIDE)



**FIGURE 5B: ADA PRIORITIZATION MAP BY EXISTING CONDITION (TOWN CENTER)**





## 3.2 Public Complaint Process

The public complaint process is an integral part of this plan. Public complaints or requests may drive the prioritization of improvements. To file a complaint or a request regarding accessibility of a sidewalk or curb ramp, submit a formal grievance in writing to the Town's ADA Coordinator with the location, date, and description of the problem. The ADA Coordinator, within 60 days of the filing of the complaint, will conduct and complete an investigation, including, if necessary, a meeting with the complainant to discuss the complaint and possible resolutions. No later than 60 days after submittal a determination shall be issued by the ADA Coordinator in writing, and where appropriate, in a format accessible to the complainant, such as large print, Braille, or audio tape. The determination will explain the position of the Town and offer options for substantive resolution of the Complaint.

**Appendix F** is a copy of the Town's public Grievance Procedure under the Americans with Disabilities Act.

## 3.3 New Construction and Alterations

To ensure the correct design of curb ramps, sidewalks, and crosswalks in new construction and alterations, the Town of Clifton Park, through the adoption of this Transition Plan, has adopted the Public Right of Way Accessibility Guidelines (PROWAG) as finalized in 2023, (hereinafter known as ADA Guidelines) or the most current regulatory guidelines in the event PROWAG becomes outdated. Once adopted by the US Department of Justice, the PROWAG will be the primary regulatory standards that govern the design and construction of all pedestrian facilities. It is important to note that in alteration projects, whenever pedestrian facilities cannot fully meet the standards outlined in the ADA Guidelines, a determination of each structural implacability or technical infeasibility must be thoroughly documented in the permanent project record.

The Town of Clifton Park's commitment to ensuring the accessibility of facilities in the public right of way is clearly demonstrated by the Work Plan (Appendix D) and schedule outlined in this report.

As previously discussed under the Jurisdiction section of this report, if another jurisdiction is undertaking alterations of a roadway, including repaving, that agency is responsible for the replacement of removed pedestrian facilities or for the construction or rehabilitation of a curb ramp to the adjacent sidewalk if an accessible curb ramp does not exist.

As described in NYSDOT's current Transition Plan, NYSDOT undertakes on-going road maintenance and alterations to ensure accessibility of pedestrian facilities on State owned roads. For the purposes of this Transition Plan, areas lacking the provision of curb ramps in State owned rights-of-way will be identified in this plan as being undertaken by NYSDOT. The improvement of these facilities will correspond to the approved NYSDOT capital improvement program.

Nothing contained in this work plan would in any way prevent the Town from modifying the implementation schedule.

### 3.4 Cost and Schedule

The detailed evaluation of the Town of Clifton Park's sidewalk network identified a total of 0.5 miles of sidewalk segments rated Partially Accessible which will require repair or replacement of barriers to bring them to compliance. For the purpose of estimating construction costs, it was assumed that 15% of the Partially Accessible segment panels will require replacement.

The cost of the repairs and replacement was estimated utilizing the NYSDOT Quick Estimator Reference. This estimate is provided as Appendix C and is summarized as follows: Utilizing the scoring of facilities provided, sidewalk replacement totals \$29,700, curb ramps \$63,000, DWS \$606,000, pedestrian signals \$205,000 and crosswalks \$1,400. It should be noted that the actual costs will vary depending on the extent of repairs, actual site conditions and the date of construction.

Utilizing the prioritization method detailed above, the Town plans to address barriers identified within this plan over five years. As identified during the location prioritization process, pedestrian facilities in the public right of way adjacent to government, community service and health facilities, the Town Center, schools and the senior center, and residential areas will be prioritized before facilities serving other areas. Curb ramps, crossings, and pedestrian signals will be addressed with their corresponding sidewalk segments or during road paving and resurfacing projects. The Town has also undergone recent projects to address some accessibility barriers within its network and has additional planned improvements underway. These projects are detailed further in **Appendix D: Work Plan and Data Inventory**.

As opportunity allows, the Town will make efforts to collaborate with stakeholders to improve the ADA accessibility of pedestrian facilities in the public right-of-way. Compliance is required to the extent feasible within the scope of the project. There will be times when it is technically infeasible to provide full compliance with the guidelines for example, if clear space at the top of the ramp is obstructed by a building or the slope of a hill is so extreme as to prevent a reasonable slope for a ramp in both directions. The inventory process may not account for such situations and could show a high-priority rating when all practical actions have been taken.

## 4.0 MONITORING AND UPDATES

### 4.1 Transition Plan Management

As with any effective planning document, the Town of Clifton Park's Transition Plan will require continued review and updating. It is recommended that the Town review and update this document on a five-year cycle. However, this does not preclude the Town from altering

that five-year cycle. If new regulations are adopted or new information is available that does not alter the intent of the transition plan, the Town intends to add appendices to the document reflecting those changes. Members of the public that wish to see changes made to the plan are encouraged to submit their comments and concerns, in writing, to the ADA Coordinator to consider including during the next update.

In addition, the Town will document progress made annually, for public review and comment. Members of the public that wish to see changes made to the plan are encouraged to submit their comments and concerns, in writing, to the ADA Coordinator to consider including during the next update.

## 4.2 Coordination with Public Improvements and Capital Expansion

The Town has identified a schedule for improvement to non-compliant facilities (Appendix D). However, this schedule may be modified to respond to complaints received by the Town. The Town should also take advantage of regular capital improvement projects or private development to upgrade deficient facilities, if occurring adjacent to those facilities.

The Town will coordinate with the New York State Department of Transportation (NYSDOT) capital improvement schedule to replace or upgrade non-compliant curb ramps and sidewalks along State highways. Likewise, the Town will coordinate with the County to replace or upgrade non-compliant curb ramps and sidewalks along County roads.

# 5.0 PUBLIC INPUT

The Town of Clifton Park provided opportunities for individuals to comment and provide input on this Transition Plan, which includes:

- Early results of the field assessment and prioritization framework were presented to the Town's Advisory Committee for feedback.
- The plan will be presented to the Town of Clifton Park Town board on DATE, and board members will be encouraged to provide feedback on behalf of their constituents.
- Copies of the draft Plan will be made available for public review and comment at Town Hall, and the library.
- The ADA Transition Plan has also been made available on the Town's website at [www.cliftonpark.org](http://www.cliftonpark.org) and
- The Town of Clifton Park will open a public comment period from Date to Date. Members of the public are encouraged to provide feedback by calling and/or emailing Town staff to log their comments.

## 5.2 Comments Received

Comments received will be included here.

# APPENDIX A: EVALUATION CRITERIA

# ADA TRANSITION PLAN

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## Appendices

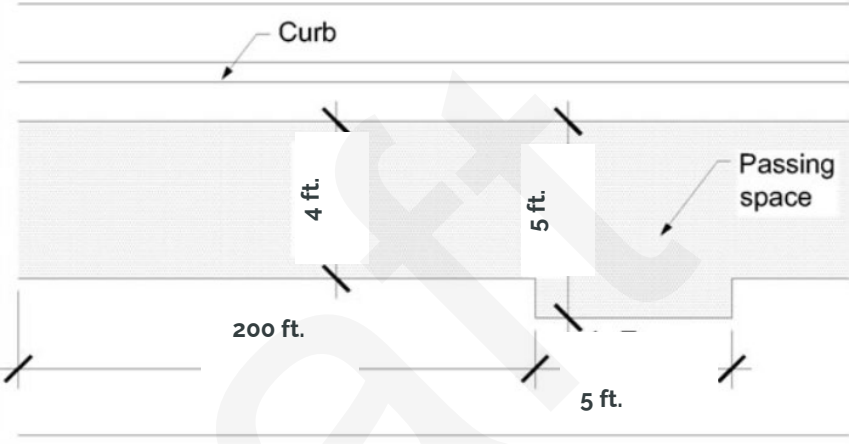

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## APPENDIX A.1: PEDESTRIAN ACCESS ROUTE

Pedestrian Access Routes (PAR) are walkways, or paths, for people to travel. The path must be a continuous width of at least 4ft. wide. If the clear width of the path is less than 5ft, then there must be a section that is at least 5ft at a maximum of every 200ft. This is referred to as a "Passing Space". The Surface of the path needs to be firm, stable, and slip resistant. Examples of commonly used materials are concrete, asphalt, and brick.

<p><i>Pedestrian Access Route/Pathway</i></p>	 <p>The diagram illustrates a cross-section of a pedestrian access route. It shows a path with a minimum width of 4 feet. A 'Curb' is indicated on the left side. A 'Passing space' is shown as a section where the path width increases to 5 feet. This passing space is required at a maximum interval of 200 feet. The diagram also shows a 5-foot wide section of the path.</p>
<p><i>Grate in Pathway:</i></p> <p>If a grate is incorporated into a pathway, the holes in the grate should be no larger than ½ inch in diameter.</p>	 <p>The photograph shows a square grate set into a paved surface. Annotations include: '1/2" Max' with a vertical arrow pointing to the height of the grate's openings; 'Long Opening' with a horizontal arrow pointing to the length of the grate's openings; and 'Pedestrian Travel' with a white arrow pointing upwards through the grate.</p>

## APPENDIX A.2: PAR AT CROSSINGS

When crossing an intersection, the pathway does NOT need to have a crosswalk visibly marked. It only requires a curb ramp on both sides of the street. If there is a cross walk, it should be maintained as explained in Appendix B.1 and B.2. When crossing over train tracks, the gap between the rails and crossing surface should be no wider than 2.5in. for commercial trains, and 3in for freight trains.

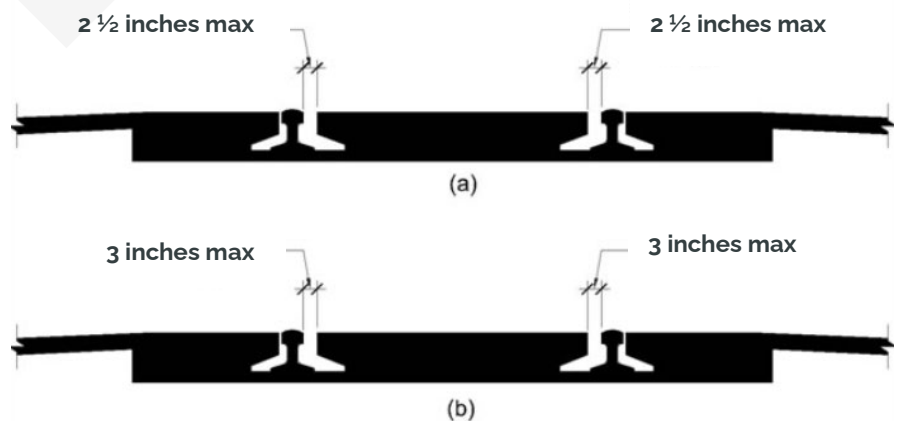
### *Street Crossings:*

While often marked, it is not required. The location of a curb ramp on both sides' makes a complete pedestrian access route. If there is a curb ramp on one side and not the other, then the path is considered inaccessible.



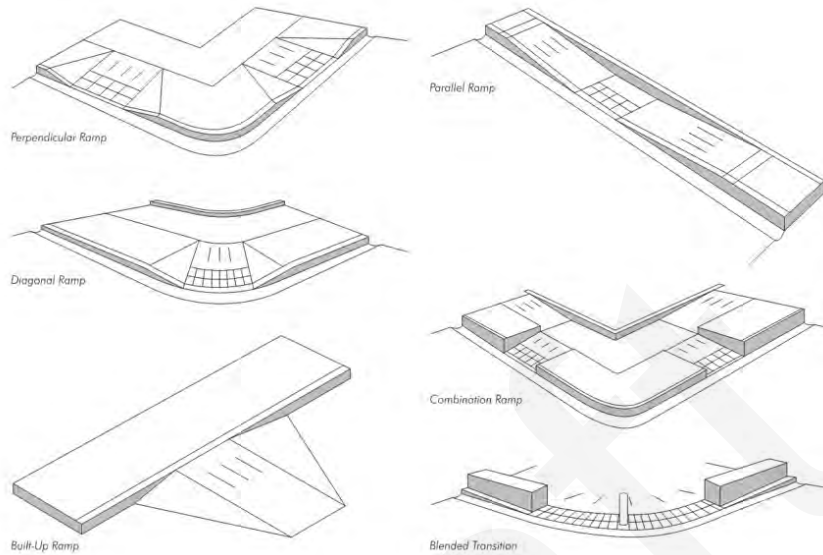
### *Railroad Crossing:*

When crossing over train tracks, the gaps are left for the train to be able to roll through. The tracks are to be at the same height as the crossing made available.



## APPENDIX A.3: CURB RAMPS

There are many different types of curb ramps that can be used for different types of intersections.



Additionally, curb ramp options shown below can be incorporated into a design as an alternate approach to traditional curb ramps.

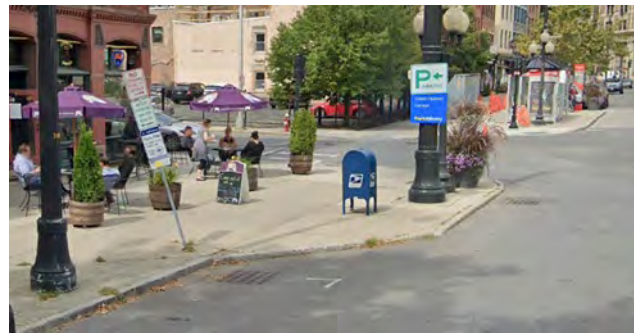
### *Pedestrian Refuge Island:*

An area within an intersection where people wait for a break in traffic to safely cross the remainder of the intersection. Curb ramps in refuge islands must meet all the accessibility criteria of curb ramps not in refuge islands.



### *Curb Extension:*

Curb extensions, also known as bulb-outs or bump outs, provide a clear view of oncoming traffic prior to leaving the sidewalk. Curb ramps on curb extensions must meet all accessibility criteria of curb ramps not on extensions.



## APPENDIX A.4: DETECTABLE WARNING SURFACE (DWS) TYPE

**Truncated Domes:** Elevated, bubble-like domes on the surface of the DWS. The domes should have an approximate height of 0.2 (1/5<sup>th</sup>) inches. The domes should face the same direction as the sidewalk.

**Pavement Grooves:** Grooves cut into the existing concrete, facing the same direction as the sidewalk. Pavement grooves are common but are not recognized as an acceptable DWS type because they do not provide a visual contrast between the pavement and ramp.



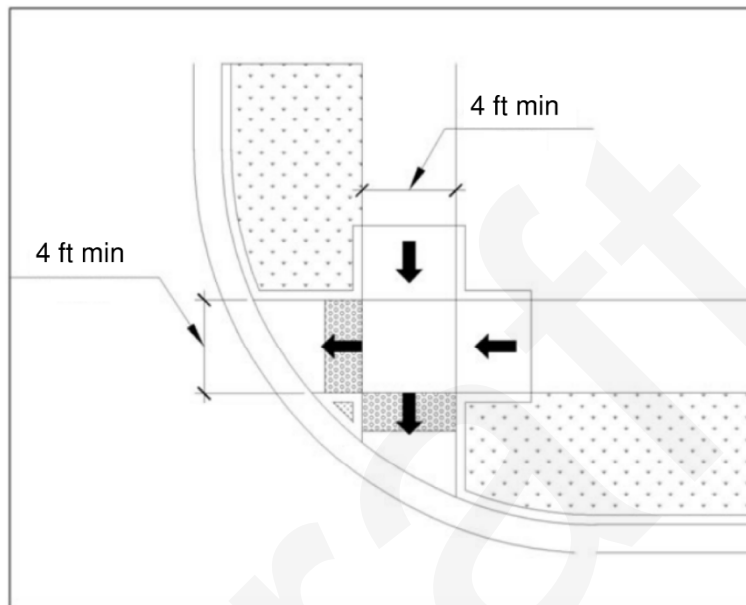
Truncated domes



Pavement grooves

## APPENDIX A.5: DETECTABLE WARNING SURFACE (DWS) WIDTH, DEPTH, PLACEMENT, AND DETERIORATION

**Width:** The DWS should extend the full width of the curb ramp or sidewalk. The minimum width is 4 feet (48 inches).

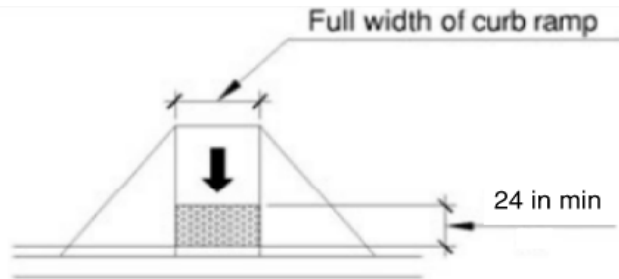


Design Guideline

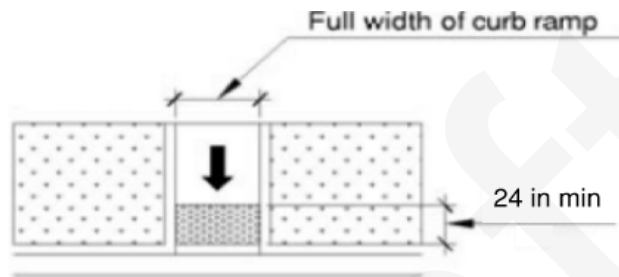


Example

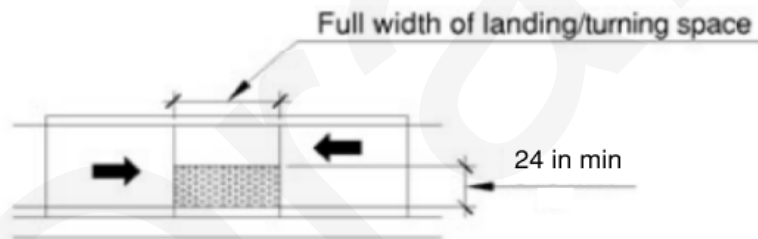
**Depth:** The DWS should be a minimum of 2 feet (24 inches) deep, in the direction of travel.



(a) perpendicular curb ramp with sloped edges



(b) perpendicular with rounded curb ramp

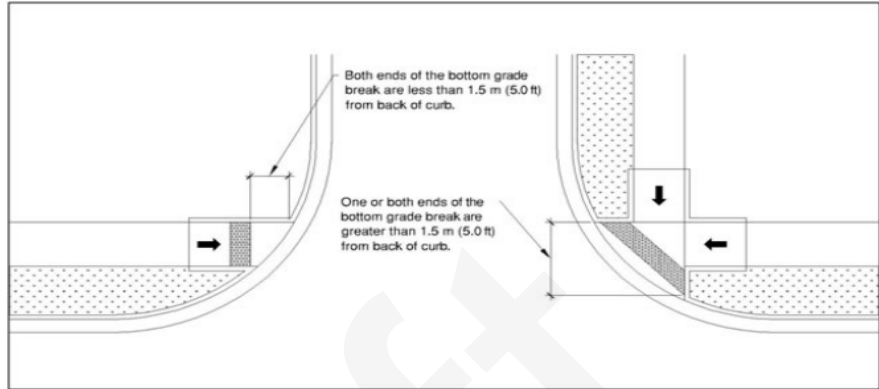


(c) parallel with curb ramp

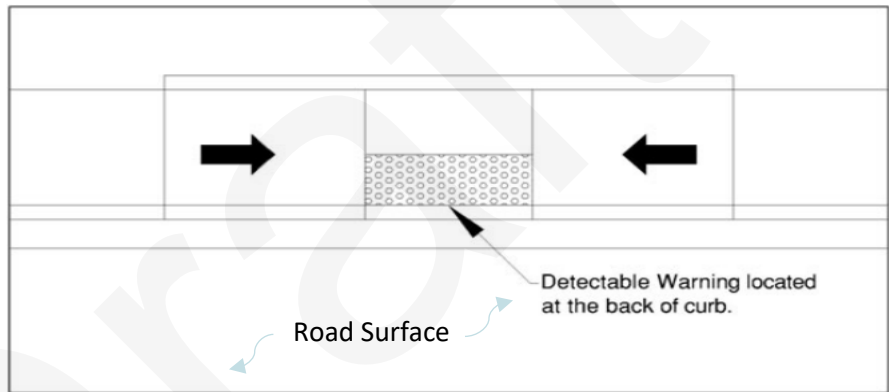
**Placement:** The DWS should face the same direction as the sidewalk unless the curb ramp serves two or more directions of crossing. Refer to the images below for the appropriate curb ramp.

*Perpendicular curb ramp*

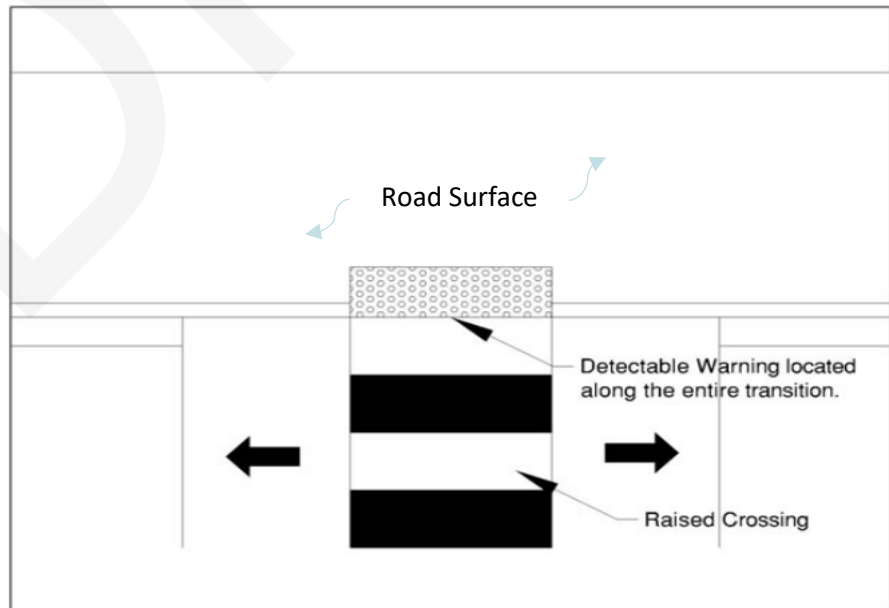
*(Angled ramps are only acceptable when serving two directions at once. There must be a ramp a person can access in both directions.)*



*Parallel curb ramp*



*Blended transition*



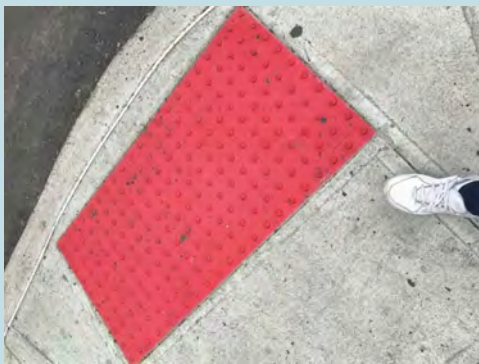
**Deterioration:** The DWS should be extruding from the surface so that visually impaired individuals will feel the strip.

<b>DWS IS NOT SIGNIFICANTLY DETERIORATED</b>	<b>DWS IS SIGNIFICANTLY DETERIORATED</b>
	

## APPENDIX A.6: CONTRASTING DWS COLOR

- The color should contrast from the sidewalk, it should at least be light-on-dark or dark-on-light.
- Best practices is to install a yellow DWS (as seen on bottom left image) as it is the most visible for the vision impaired, darker colors often appear as missing gaps in the sidewalk.

### DWS COLOR CONTRASTS FROM CURB RAMP



### DWS COLOR DOES NOT CONTRAST FROM CURB RAMP



## APPENDIX A.7: TYPE OF PEDESTRIAN SIGNAL

**Pedestrian Sign – Not pedestrian activated:** A sign marked for pedestrian crossing without a button to press. It may or may not have flashing lights.



**Pedestrian Signal – Pedestrian activated:** A button pushed by the pedestrian, which activates a crossing signal. It may or may not have a flashing light.



### Features of the pedestrian crossing buttons:

**Visual:** Flashing lights, large/bold fonts and arrows are used.

**Audible:** Locating tones, or beeps, are emitted from the signal post to guide someone to the button. Alternatively, when pressed the signal will speak, communicating to the pedestrian when it is safe to cross.

**Both:** When a signal button uses both auditory and visual cues to aid a pedestrian in crossing the street. This includes any combination of visually high contrast signage, flashing lights/signals, or audible tones/speech.

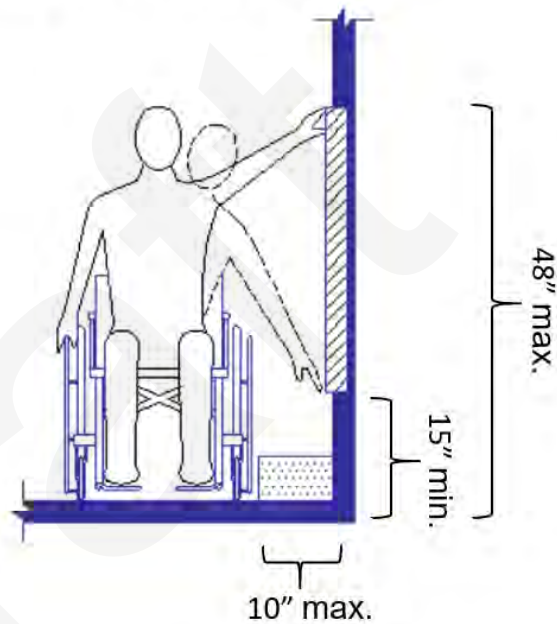
## APPENDIX A.8: PEDESTRIAN SIGNAL COMPLIANCE

When the pedestrian signal includes a button, the button should be mounted within 15 inches to 48 inches (4 feet) above the ground. The button should be accessible with a maximum of a 10 inches side reach.

### Accessible Pedestrian Pushbuttons

#### Reach Ranges

- 48" max. above the ground
- 15" min. above the ground
- Side reach within 10"



## APPENDIX B.1: CROSSWALK MATERIAL

Crosswalk markings should be smooth and even with the road surface. The material of the crosswalk may differ from the typical pavement striping for aesthetic purposes. Common materials used, other than paint, are brick pavers, stamped concrete, or asphalt.

**Painted**



**Brick Pavers**



**Stamped Concrete**



**Asphalt**



## APPENDIX B.2: CROSSWALK RATING

Rating	Condition	Reference Images
Poor	Crosswalk is heavily faded, completely faded, or missing sections.	
Fair	Crosswalk is fading but still visible.	
Good	Crosswalk is in like-new condition with minor cracks.	
Excellent	Crosswalk is new with no cracks or faults.	

## APPENDIX C.1: SIDEWALK DEFECTS

Missing Panels



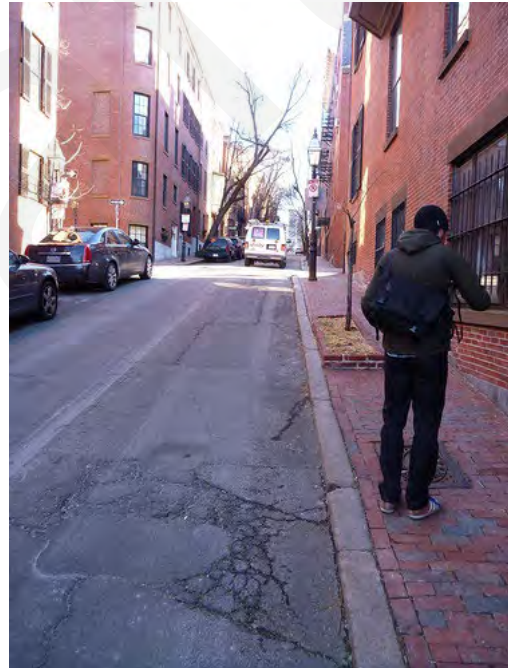
Heaving Panels





Obstructions



Too Narrow



## APPENDIX C.2: SIDEWALK SEGMENT RATING

Rating	Condition	Reference Images
Fully Accessible	<ul style="list-style-type: none"> <li>• Sidewalk is new or in like-new condition.</li> <li>• Sidewalk is level, no cracks, no faults, no obstructions, and adequate width.</li> <li>• Sidewalk is at least 4 feet (48 inches) wide.</li> </ul>	
Mostly Accessible	<ul style="list-style-type: none"> <li>• Sidewalk is in almost new or good condition.</li> <li>• Sidewalk is level and may have minor cracks.</li> <li>• Sidewalk is at least 4 feet (48 inches) wide.</li> </ul>	

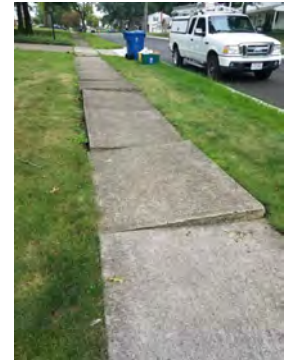
Partially  
Accessible

- Small patches of broken or chipped sidewalk, and a small buildup of dirt/debris, vegetation growing between panels or cracks.
- Sidewalk may have small vertical faults larger than  $\frac{1}{2}$  inch but could potentially be grind down/repaired.
- Large cracks are present.
- Sidewalk is less than 4 feet wide.



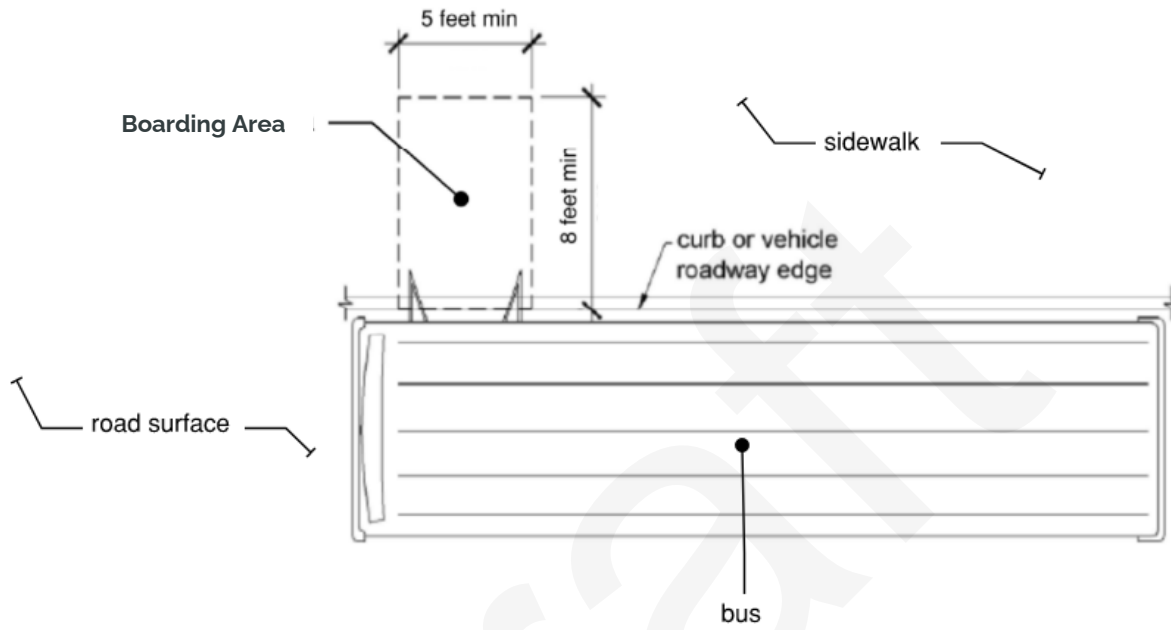
Not  
Accessible

- Sidewalk may have serious accumulation of dirt/debris, establishment of vegetation or other obstructions.
- Sidewalk is uneven from faulting (greater than  $\frac{1}{2}$  inch) that cannot be grind down/repaired and will require replacement.
- Sidewalk is less than 4 feet wide.

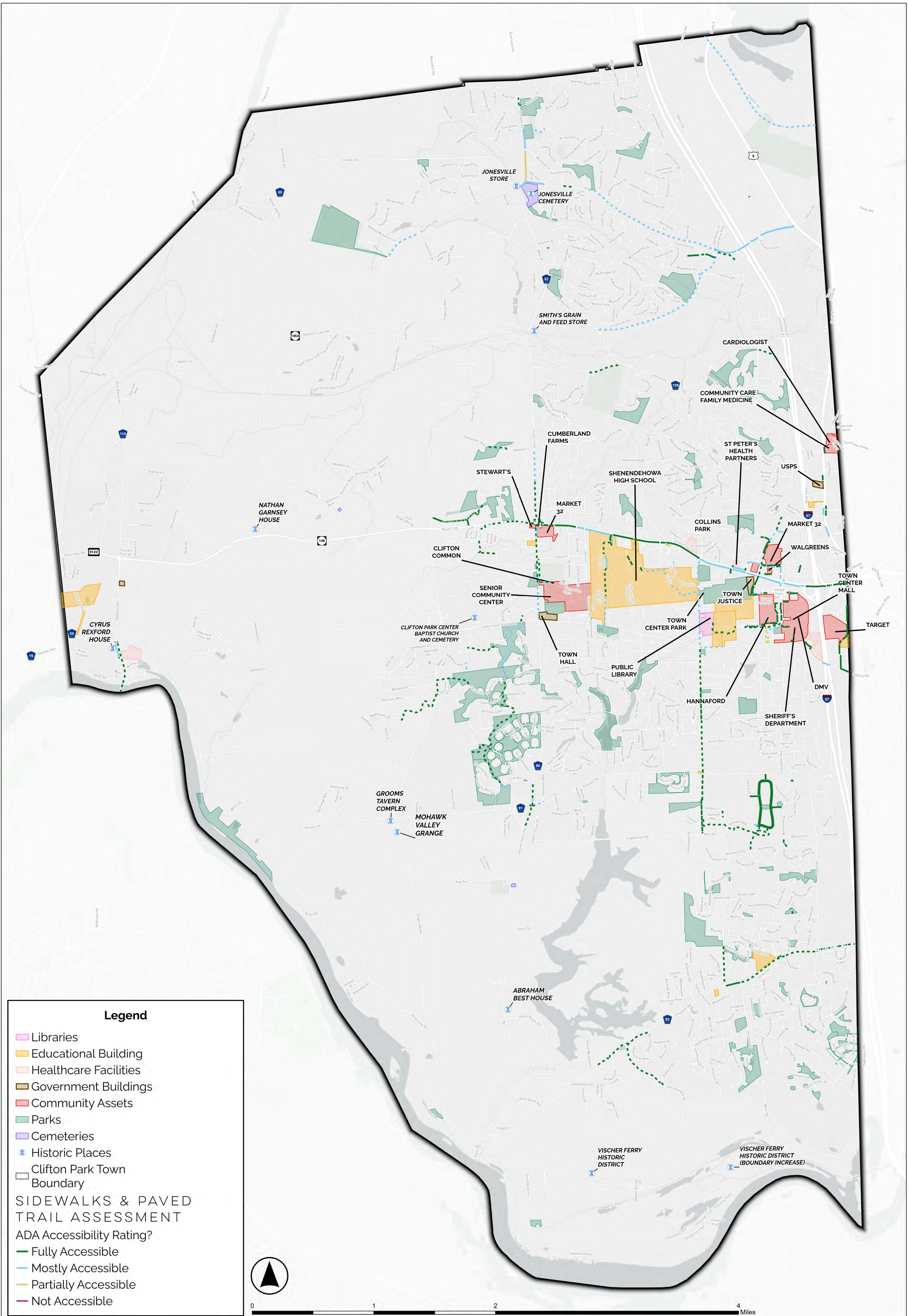


## APPENDIX D: TRANSIT STOPS

Accessible transit stops require 5 feet (60 inch) wide and 8 feet (96 inch) deep boarding area.



# APPENDIX B: MAPS



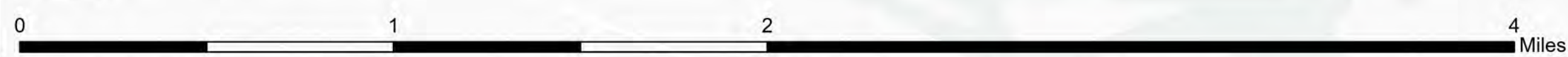
**Legend**

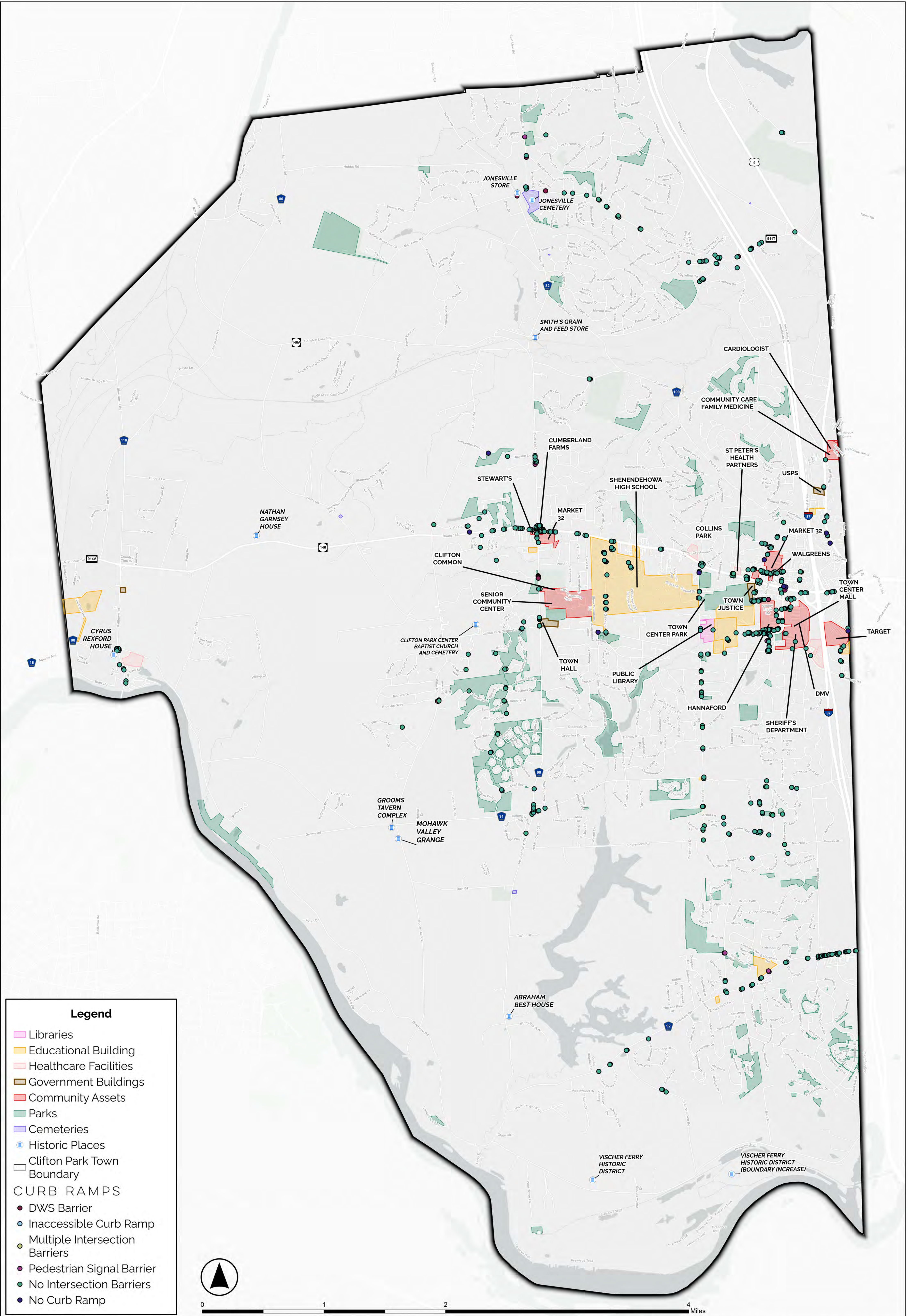
- Libraries
- Educational Building
- Healthcare Facilities
- Government Buildings
- Community Assets
- Parks
- Cemeteries
- Historic Places
- Clifton Park Town Boundary

**SIDEWALKS & PAVED TRAIL ASSESSMENT**

ADA Accessibility Rating?

- Fully Accessible
- Mostly Accessible
- Partially Accessible
- Not Accessible





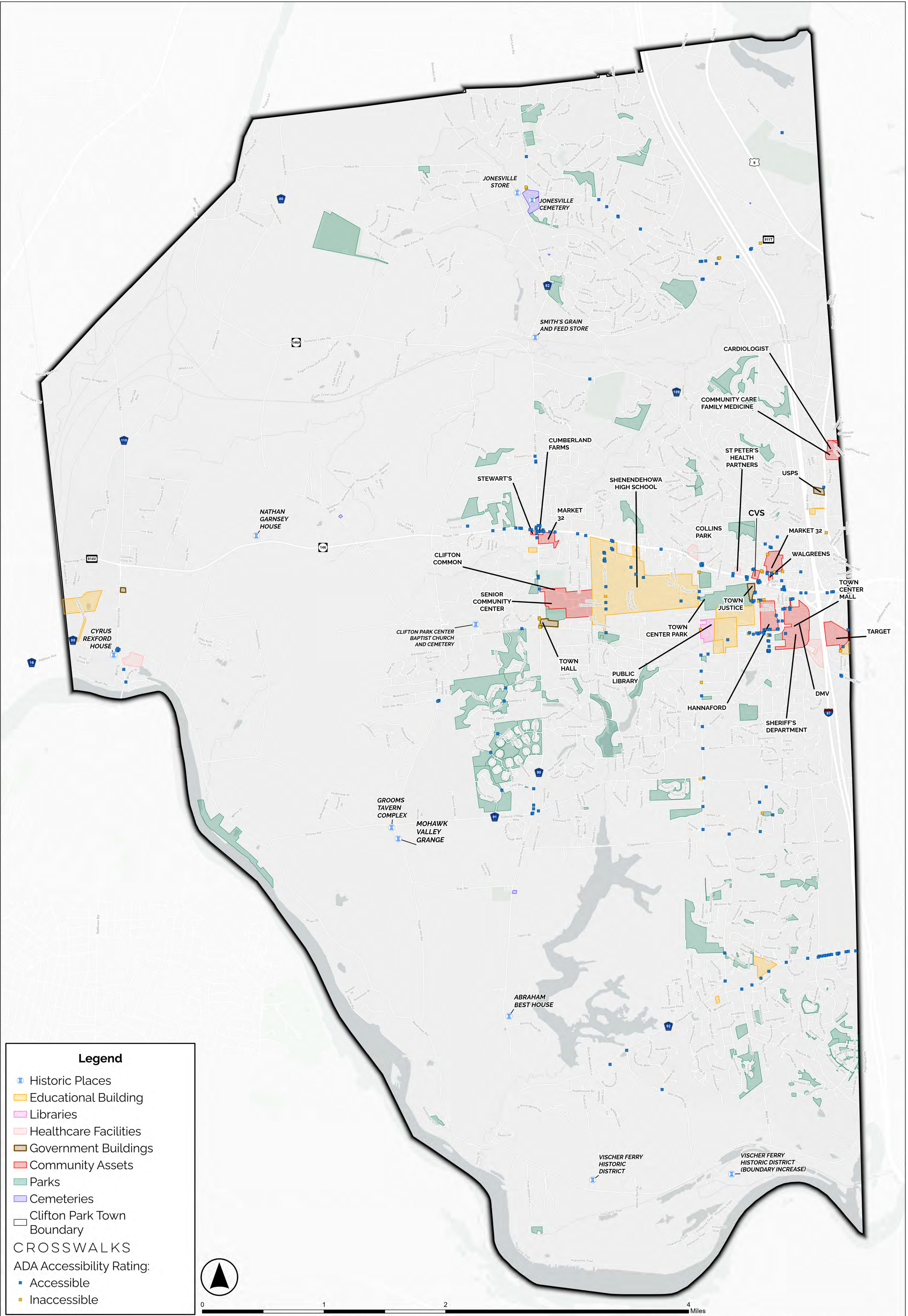
**Legend**

- Libraries
- Educational Building
- Healthcare Facilities
- Government Buildings
- Community Assets
- Parks
- Cemeteries
- Historic Places
- Clifton Park Town Boundary

**CURB RAMPS**

- DWS Barrier
- Inaccessible Curb Ramp
- Multiple Intersection Barriers
- Pedestrian Signal Barrier
- No Intersection Barriers
- No Curb Ramp



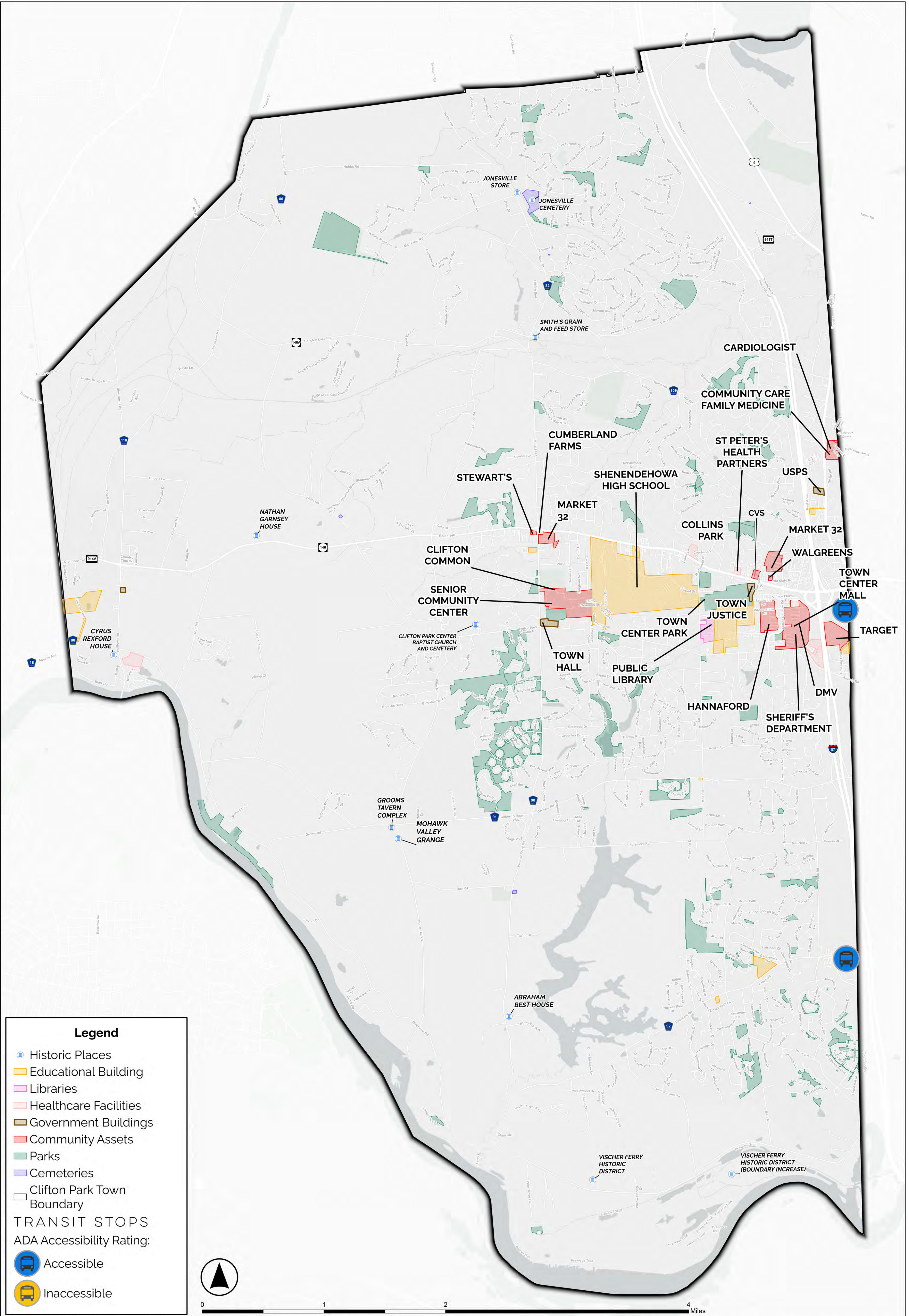


**Legend**

- Historic Places
- Educational Building
- Libraries
- Healthcare Facilities
- Government Buildings
- Community Assets
- Parks
- Cemeteries
- Clifton Park Town Boundary

**CROSSWALKS**  
 ADA Accessibility Rating:  
 Accessible  
 Inaccessible



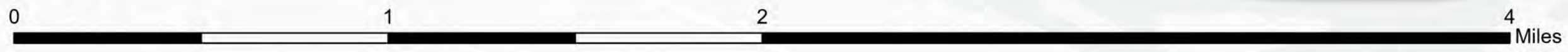


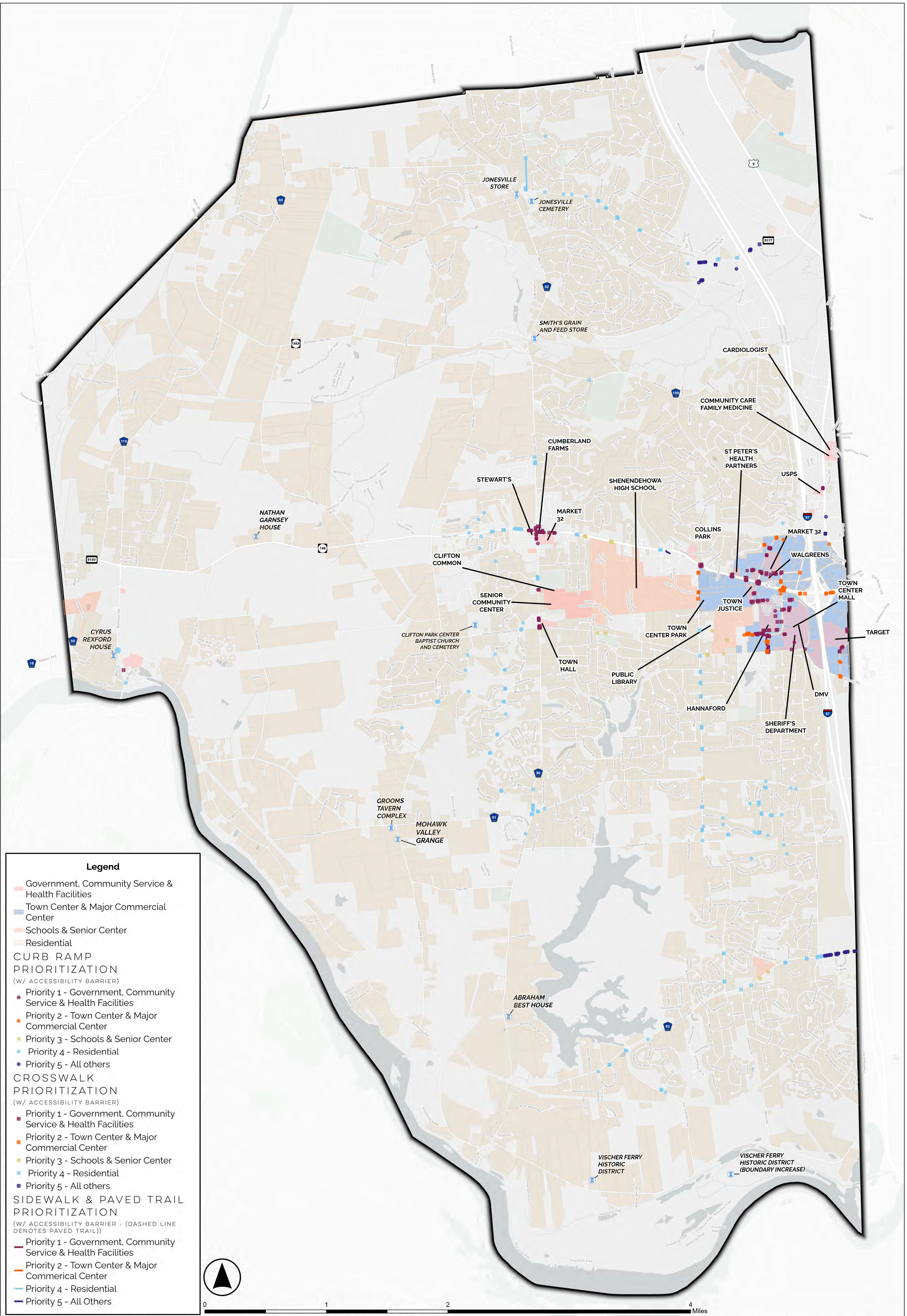
**Legend**

- Historic Places
- Educational Building
- Libraries
- Healthcare Facilities
- Government Buildings
- Community Assets
- Parks
- Cemeteries
- Clifton Park Town Boundary

**TRANSIT STOPS**  
 ADA Accessibility Rating:

- Accessible
- Inaccessible





**Legend**

- Government, Community Service & Health Facilities
- Town Center & Major Commercial Center
- Schools & Senior Center
- Residential

**CURB RAMP PRIORITIZATION**  
(W/ ACCESSIBILITY BARRIER)

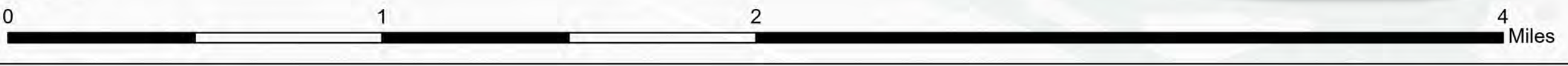
- Priority 1 - Government, Community Service & Health Facilities
- Priority 2 - Town Center & Major Commercial Center
- Priority 3 - Schools & Senior Center
- Priority 4 - Residential
- Priority 5 - All others

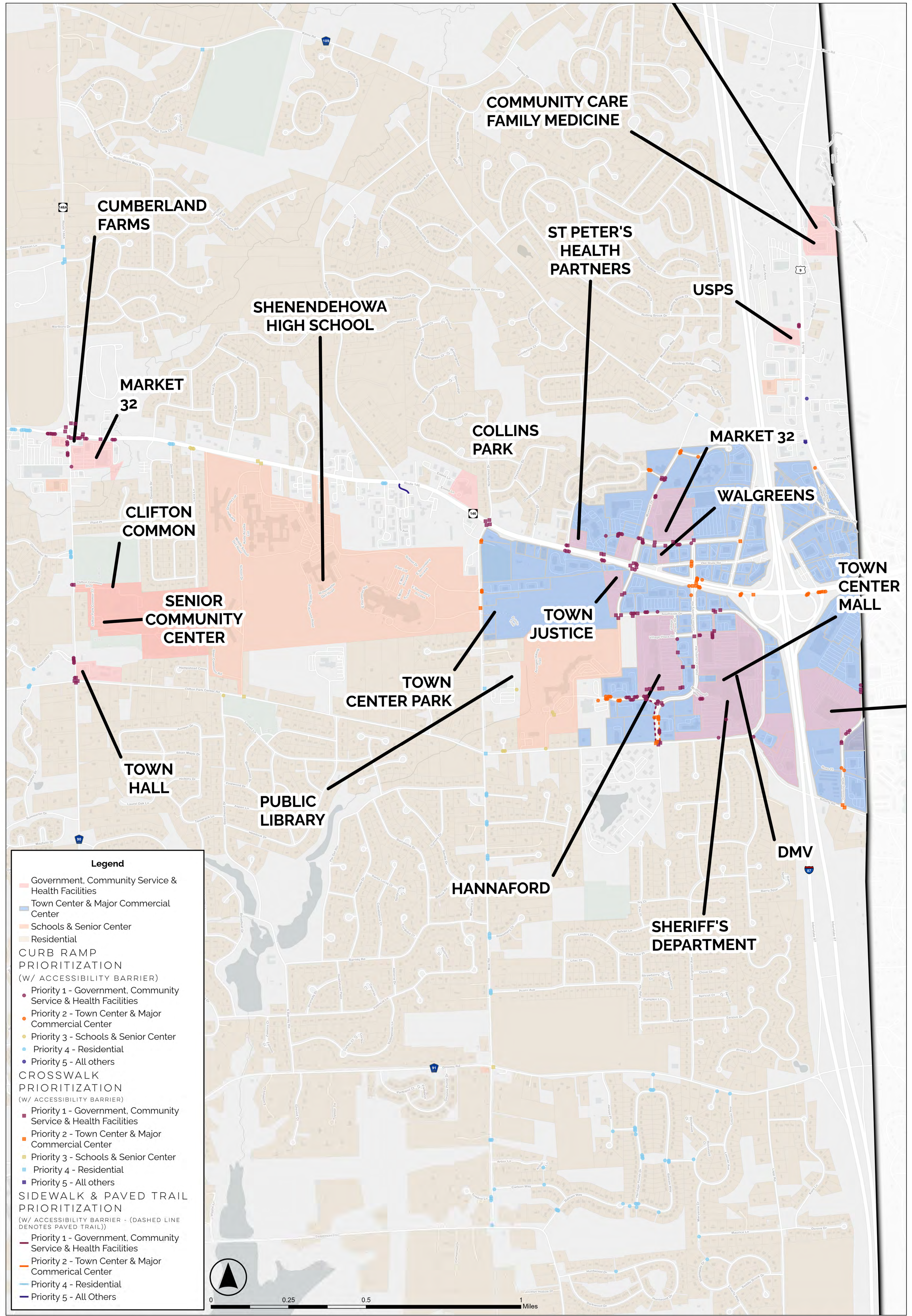
**CROSSWALK PRIORITIZATION**  
(W/ ACCESSIBILITY BARRIER)

- Priority 1 - Government, Community Service & Health Facilities
- Priority 2 - Town Center & Major Commercial Center
- Priority 3 - Schools & Senior Center
- Priority 4 - Residential
- Priority 5 - All others

**SIDEWALK & PAVED TRAIL PRIORITIZATION**  
(W/ ACCESSIBILITY BARRIER - (DASHED LINE DENOTES PAVED TRAIL))

- Priority 1 - Government, Community Service & Health Facilities
- Priority 2 - Town Center & Major Commercial Center
- Priority 4 - Residential
- Priority 5 - All Others





**Legend**

- Government, Community Service & Health Facilities
- Town Center & Major Commercial Center
- Schools & Senior Center
- Residential

**CURB RAMP PRIORITIZATION (W/ ACCESSIBILITY BARRIER)**

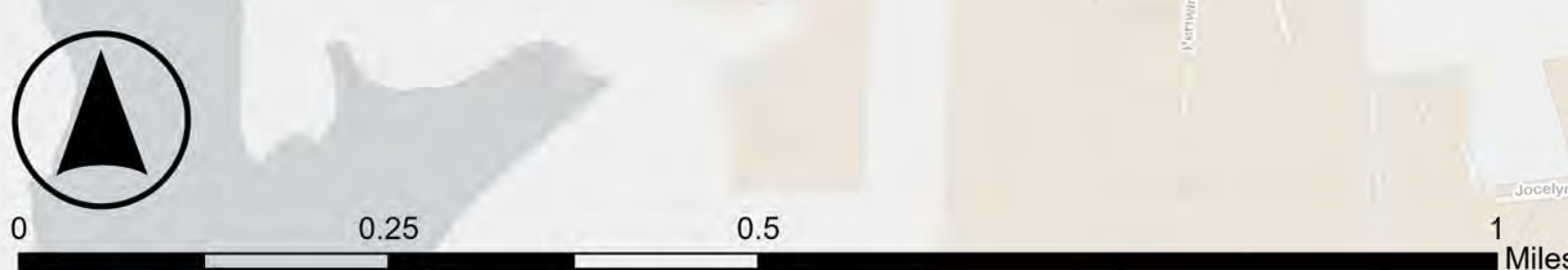
- Priority 1 - Government, Community Service & Health Facilities
- Priority 2 - Town Center & Major Commercial Center
- Priority 3 - Schools & Senior Center
- Priority 4 - Residential
- Priority 5 - All others

**CROSSWALK PRIORITIZATION (W/ ACCESSIBILITY BARRIER)**

- Priority 1 - Government, Community Service & Health Facilities
- Priority 2 - Town Center & Major Commercial Center
- Priority 3 - Schools & Senior Center
- Priority 4 - Residential
- Priority 5 - All others

**SIDEWALK & PAVED TRAIL PRIORITIZATION (W/ ACCESSIBILITY BARRIER - (DASHED LINE DENOTES PAVED TRAIL))**

- Priority 1 - Government, Community Service & Health Facilities
- Priority 2 - Town Center & Major Commercial Center
- Priority 4 - Residential
- Priority 5 - All Others





# APPENDIX C: COST ESTIMATES

Unit cost estimates are established using replacement costs from the NYSDOT Quick Estimator Reference – Upstate. Unit cost estimates are used in conjunction with field data to estimate repair costs. Note that specific costs may vary due to site conditions that may require relocating utilities/mailboxes; alteration of drainage structures; driveway aprons; acquiring right-of-way; pruning/clearing & grubbing; planting. Typical sidewalk replacement costs (2023) are observed at \$13 to \$15 per square foot, or \$65 to \$75 per linear foot (LF), for the purpose of budgeting in this report \$75 is recommended to assess cost. Additionally, based on jurisdiction outlined in Table 1 of this report, Responsible Entity for Maintenance of Pedestrian Facilities, costs of repairs for curb ramps, pedestrian signals, and crosswalks below only include right of ways under the jurisdiction and responsibility of the Town.

**Table 1 Unit Cost Estimates**

Facility	Cost
Concrete Sidewalk (4" thick, 5' wide)	\$75/linear foot
ADA Curb Ramp	\$6,300
DWS	\$2,000
LS Type Ladder Crosswalk (700ft Typical)	\$1,400
Pedestrian Signal	\$8,200

**Table 2 Town of Clifton Park ADA Compliance Estimated Costs**

Sidewalks Estimated Cost for Compliance (Obstructions Excluded):	
Partially Accessible (2,640 ft)*	\$ 29,700.00
Not Accessible (0.0 ft)	\$ 0.00
<b>Total Non-ADA Compliant Segments (12,392.76 ft)</b>	<b>\$ 29,700.00</b>
<i>*Assumed 15% of panels in need of replacement</i>	
Curb Ramps and Signals Estimated Costs for Compliance:	
10 Curb Ramps*	\$ 63,000.00
303 DWS	\$ 606,000.00
25 Pedestrian Signals	\$ 205,000.00
<b>Total Intersections:</b>	<b>\$ 874,000.00</b>

*\*Excludes "Not Flush with Asphalt"*

**Crosswalks Estimated Costs for Compliance:**

<b>Total Crosswalks*</b>	<b>\$1,400</b>
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*\*Assumed incomplete access route is addressed in curb ramp replacements*

<b>Total Capital Investment</b>	<b>\$ 905,100.00</b>
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# APPENDIX D: WORK PLAN AND DATA INVENTORY

The Town of Clifton Park is committed to ensuring facilities in the public right of way are accessible for everyone, including people with disabilities. The Town will prioritize:

**Priority 1: Government, Community Service and Health Facilities**

**Priority 2: Town Center and Major Commercial Center**

**Priority 3: Schools and Senior Center**

**Priority 4: Residential Areas**

**Priority 5: All Other**

The facilities that are prioritized first are most likely to be frequented by residents and visitors and are essential for the Town's continued operation and success. A map of all sidewalk segments by priority ranking is in Appendix B.

The "Sidewalk Segments in Priority Areas" table within this appendix will be used to identify sidewalk segments eligible for repair/replacement based on their priority ranking. Sidewalk segments will then be cross referenced with field analysis results to determine specific barriers to accessibility and the repair/replacement needs for all facilities associated with the segment (a guide to using the field analysis results is included within this appendix). The Town will then work to remove barriers to accessibility until each segment is brought to compliance, beginning with the highest priority (1), and ending with the lowest priority (4). For example: sidewalk segments ranked in priority category 1 will be first to be repaired/replaced, followed by segments ranked in category 2, and so on. The same process will be used to bring intersections, and crossings, to compliance using the corresponding tables within this appendix.

The Town may deviate from this framework to take advantage of regular capital improvement projects or private development to upgrade deficient facilities, if occurring adjacent to those facilities. For example: a low priority that is rated "Not Accessible" may be brought to compliance in conjunction with an adjacent road resurfacing project before the Town has finished bringing higher priority segments to compliance.

As previously discussed under the Jurisdiction section of this report, if another jurisdiction is undertaking alterations of a roadway, that agency is responsible for the replacement of removed pedestrian facilities or for the construction or rehabilitation of an accessible curb ramp to the adjacent sidewalk if one does not exist. The NYS Department of Transportation (NYSDOT) has completed a Transition Plan including sidewalks along state roadways and undertakes on-going road maintenance and alterations. For the purposes of this Transition Plan, areas lacking the provision of curb ramps in state rights-of-way will be identified in this

plan as being undertaken by NYSDOT. The improvement of these facilities will correspond to the approved NYSDOT capital improvement program.

Included here is a summary of recently completed work and planned work that will address some of the barriers identified in this plan.

NYS Route 146 Sidewalk System (Northcrest to Maxwell Drive) – Construction completed 10/2024 addressed ADA transition deficiencies identified and inventoried in 08/2024 along the local roads at the intersection with Route 146. The sidewalk installation construction costs, which included ADA compliance at local road intersections with Route 146 is \$474,000.00 awarded to James H. Malloy Construction.

In addition, the Town in January 2023 submitted closeout paperwork for its Pedestrian Action Safety Grant, which had a total construction budget of \$563,084.59 for the following completed improvements:

- New Ped Signals with Countdown Times and Pushbuttons will be installed at each of the signalized intersections.
- All crosswalks will be replaced with high-visibility marking & curb ramps as necessary.
- Signalized Intersections Include:
  - Intersection of Clifton Park Center Road, Clifton Country Road and Hollandale Lane – There will be 4 new crosswalks added at this location, and the median on the north approach will be extended to provide a pedestrian refuge.
  - Intersection of Clifton Park Center Road and Sitterly Road – There is one existing crosswalk at this intersection that will be upgraded.
  - Intersection of Clifton Park Center Road and Moe Road – There is one existing crosswalk at this intersection that will be upgraded.
  - Intersection of Clifton Park Center Road and Vischer Ferry Road – There are four existing crosswalks at this intersection that will be upgraded. The north approach crosswalk is currently located away from the intersection and will be relocated closer to the intersection to a location that makes the most sense.
  - Intersection of Clifton Country Road and McDonough Way – There is one existing crosswalk at this intersection that will be upgraded. Review span wire loading and add east-west backplates if possible. Crossing is currently skewed and the design should look to normalize this, if possible. Future trail project will go through this intersection and design should be coordinated with what the Town is doing in the future here.

The following uncontrolled crossings were selected for funding:

- Intersection of Clifton Country Road and Wall Street – One uncontrolled crossing of Clifton Country Road is existing and will be upgraded at this location. The median will be extended or reconfigured to provide a pedestrian refuge. Design should consider sight distance around horizontal curve and if crosswalk needs to be relocated to

improve safety it may be moved. Solar Powered RRFB shall be installed at this location along with high visibility crosswalks and new curb ramps, if necessary.

- Clifton Country Road Mid-block Crossing – One uncontrolled crossing of Clifton Country Road is existing and will be upgraded at this location. Solar Powered RRFB shall be installed at this location along with high visibility crosswalks and new curb ramps, if necessary. Crosswalk may be shifted from its existing location to shorten pedestrian crossing width.
- Clifton Park Center Road and School Drive – One uncontrolled crossing of Clifton Park Center Rd is existing and will be upgraded at this location. Solar Powered RRFB shall be installed at this location along with high visibility crosswalks and new curb ramps. Solar powered advanced warning signs will be removed. North approach has significant tree cover which may block solar charging. Design will review this and may shift crosswalk or consider separating pushbutton and RRFB sign assembly if needed.

The table below is intended to be used as an identification tool for existing barriers and is therefore inclusive of all sidewalk segments in need of improvement, regardless of jurisdiction. For those sections falling along County or State Roads, the Town will coordinate with NYSDOT or the responsible entity to address repairs.

# Sidewalks

Accessibility Rating	Sidewalk Address	Side of Road	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Partially Accessible	Southside Dr	N	1	Yes	No	No	No
Partially Accessible	Longkill Rd	E	4	Yes	Yes	No	No
Partially Accessible	Solomon Ave	S	5	Yes	No	No	No
Mostly Accessible	Southside Dr	N	1	No	No	No	No
Mostly Accessible	Southside Dr	N	1	No	No	No	No
Mostly Accessible	Clifton Park Blvd	S	1	No	No	No	No
Mostly Accessible	Clifton Park Blvd	S	1	No	No	No	No
Mostly Accessible	Clifton Park Blvd	S	1	No	No	No	No
Mostly Accessible	Clifton Park Blvd	S	1	No	No	No	No
Mostly Accessible	Wall St	N	1	No	No	Yes	No
Mostly Accessible	Clifton Country Rd	W	1	No	No	No	No
Mostly Accessible	Park Ave	S	1	No	No	No	No
Mostly Accessible	Park Ave	S	1	No	No	No	No
Mostly Accessible	Park Ave	S	1	No	No	No	No
Mostly Accessible	Park Ave	N	1	No	No	No	No
Mostly Accessible	Park Ave	N	1	No	No	No	No
Mostly Accessible	Clifton Park Blvd	N	1	No	No	No	No
Mostly Accessible	Tallow Wood Dr	E	1	No	No	No	No
Mostly Accessible	Vischer Ferry Rd	E	1	No	No	No	No
Mostly Accessible	Balltown Rd	W	1	No	No	No	No
Mostly Accessible	Sitterly Rd	N	1	No	No	No	No
Mostly Accessible	Clifton Park Blvd	S	2	No	No	No	No
Mostly Accessible	Clifton Park Blvd	S	2	No	No	No	No
Mostly Accessible	Clifton Park Blvd	S	2	No	No	No	No
Mostly Accessible	Clifton Country Rd	E	2	No	No	No	No
Mostly Accessible		N	2	No	No	No	No
Mostly Accessible	Clifton Country Rd	W	2	No	No	No	No
Mostly Accessible	Abele Blvd	S	2	No	No	No	No
Mostly Accessible	Moe Rd	E	2	No	No	No	No
Mostly Accessible	Clifton Country Rd	W	2	No	No	No	No

Accessibility Rating	Sidewalk Address	Side of Road	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Mostly Accessible		W	2	No	No	No	No
Mostly Accessible		S	3	No	No	No	No
Mostly Accessible	Longkill Rd	E	4	No	No	No	No
Mostly Accessible	Longkill Rd	E	4	No	No	No	No
Mostly Accessible	Ushers Rd	N	4	No	No	No	No
Mostly Accessible	Ushers Rd	N	4	No	No	No	No
Mostly Accessible	Ushers Rd	N	4	No	No	Yes	No
Mostly Accessible	Clifton Park Blvd	S	4	No	No	No	No
Mostly Accessible	Balsam Way	N	4	No	No	No	No
Mostly Accessible	Ushers Rd	N	5	No	No	No	No
Mostly Accessible	Ushers Rd	N	5	No	No	Yes	No
Mostly Accessible		W	5	No	No	No	No
Mostly Accessible	Van Patten Dr	W	5	No	No	No	No
Fully Accessible	Southside Dr	N	1	No	No	No	No
Fully Accessible		S	1	No	No	No	No
Fully Accessible	Clifton Park Blvd	S	1	No	No	No	No
Fully Accessible	Clifton Park Blvd	S	1	No	No	No	No
Fully Accessible	Wall St	N	1	No	No	No	No
Fully Accessible	Maxwell Dr	E	1	No	No	No	No
Fully Accessible	Maxwell Dr	E	1	No	No	No	No
Fully Accessible	Old State Rd	N	1	No	No	No	No
Fully Accessible	Park Ave	S	1	No	No	No	No
Fully Accessible	Maxwell Dr	W	1	No	No	No	No
Fully Accessible	Crossing Blvd	W	1	No	No	No	No
Fully Accessible	Crossing Blvd	W	1	No	No	No	No
Fully Accessible	Crossing Blvd	W	1	No	No	No	No
Fully Accessible	Crossing Blvd	W	1	No	No	No	No
Fully Accessible		W	1	No	No	No	No
Fully Accessible	Clifton Park Center Rd	N	1	No	No	No	No
Fully Accessible	Clifton Park Center Rd	N	1	No	No	No	No
Fully Accessible	Mc Donough Way	S	1	No	No	No	No
Fully Accessible	Mc Donough Way	S	1	No	No	No	No

Accessibility Rating	Sidewalk Address	Side of Road	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Fully Accessible	Park Ave	N	1	No	No	No	No
Fully Accessible	Clifton Park Center Rd	S	1	No	No	No	No
Fully Accessible	Sitterly Rd	N	1	No	No	No	No
Fully Accessible	Old Sitterly Road	W	1	No	No	No	No
Fully Accessible	Mc Donough Way	S	1	No	No	No	No
Fully Accessible	Mc Donough Way	S	1	No	No	No	No
Fully Accessible		E	1	No	No	No	No
Fully Accessible	Park Ave	N	1	No	No	No	No
Fully Accessible	Park Ave	N	1	No	No	No	No
Fully Accessible	Clifton Country Rd	W	1	No	No	No	No
Fully Accessible		S	1	No	No	No	No
Fully Accessible		N	1	No	No	No	No
Fully Accessible	Clifton Park Blvd	S	1	No	No	No	No
Fully Accessible	Vischer Ferry Rd	E	1	No	No	No	No
Fully Accessible	Clifton Park Blvd	S	1	No	No	No	No
Fully Accessible	Clifton Park Blvd	S	1	No	No	No	No
Fully Accessible	Village Plaza Rd	W	1	No	No	No	No
Fully Accessible	Village Plaza Rd	N	1	No	No	No	No
Fully Accessible	Village Plaza Rd	N	1	No	No	No	No
Fully Accessible	Village Plaza Rd	N	1	No	No	No	No
Fully Accessible	Village Plaza Rd	S	1	No	No	No	No
Fully Accessible	Village Plaza Rd	E	1	No	No	No	No
Fully Accessible	Village Plaza Rd	E	1	No	No	No	No
Fully Accessible	Village Plaza Rd	E	1	No	No	No	No
Fully Accessible	Village Plaza Rd	E	1	No	No	No	No
Fully Accessible	Clifton Country Rd	W	1	No	No	No	No
Fully Accessible	Main St	N	1	No	No	No	No
Fully Accessible		W	1	No	No	No	No
Fully Accessible		W	1	No	No	No	No
Fully Accessible	Maxwell Dr	W	1	No	No	No	No
Fully Accessible	Southside Dr	S	1	No	No	No	No
Fully Accessible	Southside Dr	S	1	No	No	No	No
Fully Accessible	Maxwell Dr	W	1	No	No	No	No

Accessibility Rating	Sidewalk Address	Side of Road	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Fully Accessible	Clifton Park Center Rd	N	1	No	No	No	No
Fully Accessible	Clifton Corporate Pkwy	W	1	No	No	No	No
Fully Accessible	Clifton Park Blvd	S	2	No	No	No	No
Fully Accessible	Clifton Park Blvd	S	2	No	No	No	No
Fully Accessible	Clifton Park Blvd	S	2	No	No	No	No
Fully Accessible	Wall St	N	2	No	No	No	No
Fully Accessible	Wall St	N	2	No	No	No	No
Fully Accessible	Clifton Park Blvd	S	2	No	No	No	No
Fully Accessible	Crossing Blvd	E	2	No	No	No	No
Fully Accessible	855 NY 146 Parking Lot	W	2	No	No	No	No
Fully Accessible	Plank Rd	E	2	No	No	No	No
Fully Accessible	Sitterly Rd	N	2	No	No	No	No
Fully Accessible	Mooney Way	N	2	No	No	No	No
Fully Accessible	Mooney Way	N	2	No	No	No	No
Fully Accessible	Clifton Park Blvd	S	3	No	No	No	No
Fully Accessible	Clifton Park Blvd	S	3	No	No	No	No
Fully Accessible	Clifton Park Blvd	S	3	No	No	No	No
Fully Accessible	Clifton Park Blvd	S	3	No	No	No	No
Fully Accessible		E	3	No	No	No	No
Fully Accessible	Crescent Rd	N	3	No	No	No	No
Fully Accessible	Crescent Rd	N	3	No	No	No	No
Fully Accessible	Vista Dr	E	4	No	No	No	No
Fully Accessible	Vista Ct	S	4	No	No	No	No
Fully Accessible	Vista Ct	S	4	No	No	No	No
Fully Accessible	Vista Ct	S	4	No	No	No	No
Fully Accessible	Vista Ct	S	4	No	No	No	No
Fully Accessible	Vista Ct	S	4	No	No	No	No
Fully Accessible	Vista Ct	S	4	No	No	No	No
Fully Accessible	Vista Ct	S	4	No	No	No	No
Fully Accessible	Vista Ct	S	4	No	No	No	No
Fully Accessible	Vista Ct	W	4	No	No	No	No
Fully Accessible	Vista Ct	S	4	No	No	No	No
Fully Accessible	Vista Ct	S	4	No	No	No	No



Fully Accessible	Vista Ct	S	4	No	No	No	No
Accessibility Rating	Sidewalk Address	Side of Road	Priority	Heaving Panels?	Missing Panels?	Obstructions?	Too Narrow?
Fully Accessible	Arthur Ct	S	4	No	No	No	No
Fully Accessible	Heritage Pointe Dr	E	4	No	No	No	No
Fully Accessible	Balltown Rd	E	4	No	No	No	No
Fully Accessible	Riverview Rd	S	4	No	No	No	No
Fully Accessible	Balltown Rd	W	4	No	No	No	No
Fully Accessible	Balltown Rd	E	4	No	No	No	No
Fully Accessible	Crescent Rd	S	4	No	No	No	No
Fully Accessible		E	5	No	No	No	No
Fully Accessible	Fairchild Sq	N	5	No	No	No	No
Fully Accessible	Fairchild Sq	N	5	No	No	No	No
Fully Accessible		N	5	No	No	No	No
Fully Accessible		E	5	No	No	No	No

# PAVED TRAILS

Accessibility Rating	Trail Name	Priority	Material of Segment	Obstructions?	Too Narrow?
Partially Accessible	Clifton Country Road Connector Trail	1	Asphalt	No	Yes
Partially Accessible	Wall Street Path	1	Asphalt	No	Yes
Partially Accessible	Clifton Country Road Connector Trail	2	Asphalt	Yes	Yes
Partially Accessible	Clifton Country Road Connector Trail	2	Asphalt	Yes	No
Mostly Accessible	Maxwell Drive Bike Path	1	Asphalt	Yes	No
Mostly Accessible	Vischer Ferry Road Trail	1	Asphalt	No	No
Mostly Accessible	Vischer Ferry Road Trail	1	Asphalt	No	No
Mostly Accessible	Vischer Ferry Road Trail	1	Concrete	No	No
Mostly Accessible	Clifton Country Road Connector Trail	1	Asphalt	No	No
Mostly Accessible	Moe Road Bike Path	2	Asphalt	No	No
Mostly Accessible	Clifton Park Center Road Trail	3	Asphalt	No	No
Mostly Accessible	Longkill Jewett Park Trail	4	Asphalt	No	No
Mostly Accessible	Robert Van Patten Memorial Trail	4	Asphalt	No	No
Mostly Accessible	Ushers Road Trail	4	Asphalt	No	No
Mostly Accessible	MacElroy Road Trail	4	Asphalt	No	No
Mostly Accessible	Ushers Road Trail	4	Asphalt	No	No
Mostly Accessible	Vischer Ferry Road Trail	4	Concrete	No	No
Mostly Accessible	Settlers Hill Trail	4	Asphalt	No	No
Mostly Accessible	Clifton Park Central Neighborhoods Con	4	Asphalt	No	No
Mostly Accessible	Grooms Road Trail	4	Asphalt	No	No
Mostly Accessible	Route 146 Trail	4	Asphalt	No	No
Mostly Accessible	Vischer Ferry Road Trail	4	Asphalt	No	No
Mostly Accessible	Clifton Park Central Neighborhoods Con	4	Asphalt	No	No
Mostly Accessible	Zim Smith Trail	4	Asphalt	No	No
Mostly Accessible	Zim Smith Trail	4	Asphalt	No	No
Mostly Accessible	Clifton Park Central Neighborhoods Con	4	Asphalt	No	No
Mostly Accessible	NYS Route 146A Trail	4	Asphalt	No	No
Mostly Accessible	Zim Smith Trail	5	Asphalt	No	No
Mostly Accessible	Crescent Road Trail	5	Asphalt	Yes	No
Mostly Accessible	Crescent Road Trail	5	Asphalt	No	No
Fully Accessible	Vischer Ferry Road Trail	1	Asphalt	No	No

Accessibility Rating	Trail Name	Priority	Material of Segment	Obstructions?	Too Narrow?
Fully Accessible	Clifton Commons Connector Trail	1	Asphalt	No	No
Fully Accessible	Maxwell Drive Bike Path	1	Asphalt	No	No
Fully Accessible	Maxwell Drive Bike Path	1	Concrete	No	No
Fully Accessible	Maxwell Drive Bike Path	1	Concrete	No	No
Fully Accessible	Clifton Country Road Connector Trail	1	Asphalt	No	No
Fully Accessible	Village Plaza Connector Trail	1	Asphalt	No	No
Fully Accessible	Clifton Country Road Connector Trail	1	Asphalt	No	No
Fully Accessible	Wall Street Path	1	Asphalt	No	No
Fully Accessible	Wall Street Path	1	Asphalt	No	No
Fully Accessible	Clifton Commons Connector Trail	1	Asphalt	No	No
Fully Accessible	Clifton Park Center Mall Connector Trail	1	Asphalt	No	No
Fully Accessible	Route 146 Trail	1	Asphalt	No	No
Fully Accessible	Vischer Ferry Road Trail	1	Asphalt	No	No
Fully Accessible	Rexford Bridge Trail	1	Asphalt	No	No
Fully Accessible	Route 146 Trail	1	Asphalt	No	No
Fully Accessible	NYS Route 146A Trail	1	Asphalt	No	No
Fully Accessible	Maxwell Drive Bike Path	2	Asphalt	No	No
Fully Accessible	Wall Street Path	2	Asphalt	No	No
Fully Accessible	Wall Street Path	2	Asphalt	No	No
Fully Accessible	Moe Road Bike Path	3	Asphalt	No	No
Fully Accessible	Clifton Park Halfmoon Library Trail	3	Asphalt	No	No
Fully Accessible	Crescent Road Trail	3	Asphalt	No	No
Fully Accessible	Clifton Park Halfmoon Library Trail	3	Asphalt	No	No
Fully Accessible	Crescent Road Trail	3	Asphalt	No	No
Fully Accessible	Kinns Road Connector Trail	4	Asphalt	No	No
Fully Accessible	Village Green Trail	4	Asphalt	No	No
Fully Accessible	Dakota Trail	4	Asphalt	No	No
Fully Accessible	Vischer Ferry Road Trail	4	Asphalt	No	No
Fully Accessible	Clifton Park Central Neighborhoods Con	4	Asphalt	No	No
Fully Accessible	Route 146 Trail Spur	4	Asphalt	No	No

Accessibility Rating	Trail Name	Priority	Material of Segment	Obstructions?	Too Narrow?
Fully Accessible	The Vistas Connector Trail	4	Asphalt	No	No
Fully Accessible	Longkill Road Trail	4	Asphalt	No	No
Fully Accessible	Grooms Road Trail	4	Asphalt	No	No
Fully Accessible	Crescent Road Trail	4	Asphalt	No	No
Fully Accessible	Crescent Road Trail	4	Asphalt	No	No
Fully Accessible	Ushers Road Trail	4	Asphalt	No	No
Fully Accessible	Carlson Farms Connector Trail	4	Asphalt	No	No
Fully Accessible	Settlers Hill Trail	4	Asphalt	No	No
Fully Accessible	Settlers Hill Trail	4	Asphalt	No	No
Fully Accessible	Settlers Hill Trail	4	Asphalt	No	No
Fully Accessible	Arbor Lane Connector Trail	4	Asphalt	No	No
Fully Accessible	Clifton Park Central Neighborhoods Con	4	Asphalt	No	No
Fully Accessible	Clifton Park Central Neighborhoods Con	4	Asphalt	No	No
Fully Accessible	Clifton Park Central Neighborhoods Con	4	Asphalt	No	No
Fully Accessible	Clifton Park Central Neighborhoods Con	4	Asphalt	No	No
Fully Accessible	Clifton Park Central Neighborhoods Con	4	Asphalt	No	No
Fully Accessible	Vischer Ferry Road Trail	4	Asphalt	No	No
Fully Accessible	Southwick Meadows Trail	4	Asphalt	No	No
Fully Accessible	Bridlewood Trail	4	Asphalt	No	No
Fully Accessible	Bridlewood Trail	4	Asphalt	No	No
Fully Accessible	Southwick Meadows Trail	4	Asphalt	No	No
Fully Accessible	Southwick Meadows Trail	4	Asphalt	No	No
Fully Accessible	Longkill Road Trail	4	Asphalt	No	No
Fully Accessible	Route 146 Trail	4	Asphalt	No	No
Fully Accessible	Route 146 Trail	4	Asphalt	No	No
Fully Accessible	Brookhaven Park/Lane Estates Connect	4	Asphalt	No	No
Fully Accessible	Crescent Road Trail	4	Asphalt	No	No
Fully Accessible	Crescent Road Trail	4	Concrete	No	No
Fully Accessible	Crescent Road Trail	4	Asphalt	No	No
Fully Accessible	Route 146 Trail	4	Asphalt	No	No











Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Clifton Park Blvd	S	1	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Blvd	S	1	Yes	Yes	Yes	No				
No Intersection Barriers	Tallow Wood Dr	W	1	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Blvd	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NY 146	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NY 146	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Park Blvd	S	1	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Blvd	S	1	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Blvd	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Park Blvd	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Maxwell Drive Bike Path		1	Yes	Yes	Yes	No				
No Intersection Barriers	Maxwell Dr	W	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Park Ave	S	1	Yes	Yes	Yes	No				
No Intersection Barriers	Park Ave	N	1	Yes	Yes	Yes	No				
No Intersection Barriers	Park Ave	S	1	Yes	Yes	Yes	No				
No Intersection Barriers	Park Ave	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Park Ave	N	1	Yes	Yes	Yes	No				





Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Clifton Park Center Rd	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Park Center Rd	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Park Center Rd	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Park Center Rd	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Park Center Rd	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Mc Donough Way	S	1	Yes	Yes	Yes	No				
No Intersection Barriers	Mc Donough Way	S	1	Yes	Yes	Yes	No				
No Intersection Barriers	Mcdonough Way	S	1	Yes	Yes	Yes	No				
No Intersection Barriers	Mcdonough Way	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Mcdonough Way	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Mcdonough Way	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Mc Donough Way	S	1	Yes	Yes	Yes	No				
No Intersection Barriers	Mc Donough Way	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Mc Donough Way	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Country Rd	W	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Country Rd	W	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Village Plaza Rd	E	1	Yes	Yes	Yes	No				



Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Village Plaza Rd	W	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Village Plaza Rd	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Wall St	N	1	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Country Road Connector Trail		1	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Country Road Connector Trail		1	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Country Road Connector Trail		1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Wall Street Path		1	Yes	Yes	Yes	No				
No Intersection Barriers	Wall Street Path		1	Yes	Yes	Yes	No				
No Intersection Barriers	Wall Street Path		1	Yes	Yes	Yes	No				
No Intersection Barriers	Wall Street Path		1	Yes	Yes	Yes	No				
No Intersection Barriers	Wall St	N	1	Yes	Yes	Yes	No				
No Intersection Barriers	Wall St	N	1	Yes	Yes	Yes	No				
No Intersection Barriers	Wall St	N	1	Yes	Yes	Yes	No				
No Intersection Barriers	Mc Donough Way	S	1	Yes	Yes	Yes	No				



Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Rexford Bridge Trail		1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Vischer Ferry Road Trail	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NY 146	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NY 146	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NY 146	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NY 146A	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NY 146A	W	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NY 146A	W	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NYS Route 146A Trail		1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NY 146	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Park Blvd	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NY 146	N	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Park Blvd	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Park Blvd	S	1	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Route 146 Trail		1	Yes	Yes	Yes	No				
No Intersection Barriers	Route 146 Trail		1	Yes	Yes	Yes	No				
No Intersection Barriers	Route 146 Trail		1	Yes	Yes	Yes	No				





Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Plank Rd	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Plank Rd	W	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Plank Rd	W	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Plank Rd	E	2	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Country Rd	W	2	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Country Rd	W	2	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Blvd	S	2	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Country Rd	W	2	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Country Road Connector Trail		2	Yes	Yes	Yes	No				
No Intersection Barriers	Wall Street Path		2	Yes	Yes	Yes	No				
No Intersection Barriers	Wall Street Path		2	Yes	Yes	Yes	No				
No Intersection Barriers	Wall Street Path		2	Yes	Yes	Yes	No				
No Intersection Barriers	Wall Street Path		2	Yes	Yes	Yes	No				
No Intersection Barriers	Wall Street Path		2	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Country Road Connector Trail		2	Yes	Yes	Yes	No				











Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Ushers Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Ushers Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Ushers Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Longkill Rd	E	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Rt 82	W	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Longkill Rd	E	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Longkill Jewett Park Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Zim Smith Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Zim Smith Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Kinns Road Connector Trail		4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Nottingham Way N	W	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				

Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Meadow Estates to Addison Estates Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Estates to Addison		4	Yes	Yes	Yes	No				
No Intersection Barriers	Settlers Hill Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Settlers Hill Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Settlers Hill Trail		4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Settlers Hill Trail		4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Settlers Hill Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				

Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				
Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?

No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Grooms Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Balsam Way Natural Area Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Carlson Farms Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Balsam Way Natural Area Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Countryman Estates Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Countryman Estates Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Grooms Road Trail		4	Yes	Yes	Yes	No				





Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Balltown Rd	E	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Balltown Rd	W	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Balltown Rd	W	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Balltown Rd	W	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Balltown Rd	W	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Rexford Bridge Trail		4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Rexford Bridge Trail		4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Rexford Bridge Trail		4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Park Center Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Center Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				

Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				
No Intersection Barriers	NYS Route 146A Trail		4	Yes	Yes	Yes	Yes	Yes		Yes	Yes
No Intersection Barriers	NY 146A	W	4	Yes	Yes	Yes	Yes	Yes	Yes		
No Intersection Barriers	NYS Route 146A Trail		4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Emerson to Falcon Ridge Multi Use Trail		4	Yes							
No Intersection Barriers	Carlson Farms Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	No				
No Intersection Barriers	Balsam Way	S	4	Yes	Yes	Yes	No				
No Intersection Barriers	Balsam Way	S	4	Yes	Yes	Yes	No				

Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Balsam Way	S	4	Yes	Yes	Yes	No				
No Intersection Barriers	Heritage Pointe Dr	W	4	Yes	Yes	Yes	No				
No Intersection Barriers	Balsam Way	N	4	Yes	Yes	Yes	No				
No Intersection Barriers	Heritage Pointe Dr	E	4	Yes	Yes	Yes	No				
No Intersection Barriers	Heritage Pointe Dr	W	4	Yes	Yes	Yes	No				
No Intersection Barriers	Heritage Pointe Dr	E	4	Yes	Yes	Yes	No				
No Intersection Barriers	Heritage Pointe Dr	W	4	Yes	Yes	Yes	No				
No Intersection Barriers	Heritage Pointe Dr	W	4	Yes	Yes	Yes	No				
No Intersection Barriers	Arthur Ct	S	4	Yes	Yes	Yes	No				
No Intersection Barriers	Heritage Pointe Dr	N	4	Yes	Yes	Yes	No				
No Intersection Barriers	Heritage Pointe Dr	E	4	Yes	Yes	Yes	No				
No Intersection Barriers	Heritage Pointe Dr	E	4	Yes	Yes	Yes	No				
No Intersection Barriers	Lorraine Ct	N	4	Yes	Yes	Yes	No				
No Intersection Barriers	Balsam Way	W	4	Yes	Yes	Yes	No				
No Intersection Barriers	Lorraine Ct	N	4	Yes	Yes	Yes	No				
No Intersection Barriers	Balsam Way	E	4	Yes	Yes	Yes	No				
No Intersection Barriers	Route 146 Trail		4	Yes	Yes	Yes	No				

Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Route 146 Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Route 146 Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Route 146 Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Route 146 Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Route 146 Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Route 146 Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Route 146 Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Route 146 Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Route 146 Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Route 146 Trail Spur		4	Yes	Yes	Yes	No				
No Intersection Barriers	Brookhaven Park/Lane Estates Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Brookhaven Park/Lane Estates Connector Trail		4	Yes	Yes	Yes	No				
Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?

No Intersection Barriers	Clifton Park Central Neighborhoods Connector Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Blvd	S	4	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Blvd	S	4	Yes	Yes	Yes	No				
No Intersection Barriers	RT 91	N	4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Vischer Ferry Road Trail		4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Vischer Ferry Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Vischer Ferry Road Trail		4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Grooms Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Grooms Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Vischer Ferry Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Vischer Ferry Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Vischer Ferry Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Vischer Ferry Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	Yes	Yes		Yes	Yes
No Intersection Barriers	Moe Road Bike Path		4	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Ushers Road Trail		4	Yes	Yes	Yes	No				
No Intersection Barriers	Ushers Road Trail		4	Yes	Yes	Yes	No				





Accessibility Barrier	Road Name	Road Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
No Intersection Barriers	Crescent Road Trail		5	Yes	Yes	Yes	No				
No Intersection Barriers	Crescent Road Trail		5	Yes	Yes	Yes	No				
No Intersection Barriers	Crescent Road Trail		5	Yes	Yes	Yes	No				
No Intersection Barriers	Crescent Road Trail		5	Yes	Yes	Yes	No				
No Intersection Barriers	Crescent Road Trail		5	Yes	Yes	Yes	No				
No Intersection Barriers	Crescent Road Trail		5	Yes	Yes	Yes	No				
No Intersection Barriers	Crescent Road Trail		5	Yes	Yes	Yes	No				
No Intersection Barriers	Crescent Road Trail		5	Yes	Yes	Yes	No				
No Intersection Barriers	NYS Route 146A Trail		5	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NY 146	N	5	Yes	Yes	Yes	Yes	Yes	Yes	Yes	
No Intersection Barriers	NY 146A	E	5	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NY 146	N	5	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	NY 146	N	5	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	Clifton Park Blvd	S	5	Yes	Yes	Yes	No				
No Intersection Barriers	Clifton Park Blvd	S	5	Yes	Yes	Yes	No				
No Intersection Barriers	US 9	W	5	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
No Intersection Barriers	US 9	E	5	Yes	Yes	Yes	No				



# CROSSINGS

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
Crossing Barrier	Crossing Blvd	1	No		No	
Crossing Barrier	Vischer Ferry Rd	1	No		No	
Crossing Barrier	Heritage Pointe Dr	4	No		No	
Crossing Barrier	Commerce Dr	5	Yes	CracksPotholes	Yes	Good
No Crossing Barrier	Moe Rd	1	Yes		Yes	Good
No Crossing Barrier	Clifton Corporate Pkwy	1	Yes		Yes	Good
No Crossing Barrier	Clifton Corporate Pkwy	1	Yes		No	
No Crossing Barrier	Municipal Plz	1	Yes		No	
No Crossing Barrier	Municipal Plz	1	Yes		No	
No Crossing Barrier	Maxwell Dr	1	Yes		Yes	Excellent
No Crossing Barrier	Maxwell Dr	1	Yes		Yes	Excellent
No Crossing Barrier	Maxwell Dr	1	Yes		Yes	Excellent
No Crossing Barrier	Maxwell Dr	1	Yes		Yes	Excellent
No Crossing Barrier	Maxwell Dr	1	Yes		Yes	Poor
No Crossing Barrier	Maxwell Dr	1	Yes		Yes	Poor
No Crossing Barrier	Park Ave	1	Yes		No	
No Crossing Barrier	Park Ave	1	Yes		No	

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
No Crossing Barrier	Park Ave	1	Yes		No	
No Crossing Barrier	Park Ave	1	Yes		Yes	Fair
No Crossing Barrier	Park Ave	1	Yes		No	
No Crossing Barrier	Clifton Country Rd Extension	1	Yes		Yes	Fair
No Crossing Barrier	Maxwell Dr	1	Yes		No	
No Crossing Barrier	Maxwell Dr	1	Yes		No	
No Crossing Barrier	Maxwell Dr	1	Yes		No	
No Crossing Barrier	Maxwell Dr	1	Yes		Yes	Good
No Crossing Barrier	Maxwell Dr	1	Yes		No	
No Crossing Barrier	Southside Dr	1	Yes		Yes	Good
No Crossing Barrier	Southside Dr	1	Yes		Yes	Poor
No Crossing Barrier	Southside Dr	1	Yes		No	
No Crossing Barrier	Southside Dr	1	Yes		Yes	Fair
No Crossing Barrier	Clifton Park Center Rd	1	Yes		Yes	Fair
No Crossing Barrier	Clifton Park Center Rd	1	Yes		Yes	Good
No Crossing Barrier	Clifton Park Center Rd	1	Yes		Yes	Good

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
No Crossing Barrier	Mcdonough Way	1	Yes		Yes	Fair
No Crossing Barrier	Mcdonough Way	1	Yes		Yes	Good
No Crossing Barrier	Mcdonough Way	1	Yes		No	
No Crossing Barrier	Clifton Country Rd	1	Yes		Yes	Good
No Crossing Barrier	Clifton Country Rd	1	Yes		Yes	Good
No Crossing Barrier	Village Plaza Rd	1	Yes		Yes	Good
No Crossing Barrier	Village Plaza Rd	1	Yes		Yes	Good
No Crossing Barrier	Clifton Country Rd	1	Yes		Yes	Good
No Crossing Barrier	Village Plaza Rd	1	Yes		Yes	Good
No Crossing Barrier	Village Plaza Rd	1	Yes		Yes	Good
No Crossing Barrier	Village Plaza Rd	1	Yes		No	
No Crossing Barrier	Village Plaza Rd	1	Yes		Yes	Good
No Crossing Barrier	Village Plaza Rd	1	Yes		Yes	Good
No Crossing Barrier	Village Plaza Rd	1	Yes		No	
No Crossing Barrier	Clifton Country Rd	1	Yes		Yes	Poor
No Crossing Barrier	Wall St	1	Yes		No	

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
No Crossing Barrier	Wall St	1	Yes		No	
No Crossing Barrier	Wall St	1	Yes		No	
No Crossing Barrier	Wall St	1	Yes		No	
No Crossing Barrier	Wall St	1	Yes		No	
No Crossing Barrier	Mcdonough Way	1	Yes		Yes	Good
No Crossing Barrier	Crossing Blvd	1	Yes		No	
No Crossing Barrier	Crossing Blvd	1	Yes		No	
No Crossing Barrier	Park Ave	1	Yes		Yes	Good
No Crossing Barrier	Park Ave	1	Yes		No	
No Crossing Barrier	Maxwell Dr	1	Yes		Yes	Fair
No Crossing Barrier	Maxwell Dr	1	Yes		No	
No Crossing Barrier	Maxwell Dr	1	Yes		No	
No Crossing Barrier	Vischer Ferry Rd	1	Yes		Yes	Good
No Crossing Barrier	Vischer Ferry Rd	1	Yes		Yes	Good
No Crossing Barrier	Vischer Ferry Rd	1	Yes		Yes	Good
No Crossing Barrier	Route 146A	1	Yes		Yes	Good

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
No Crossing Barrier	Route 146	1	Yes		Yes	Good
No Crossing Barrier	Route 146	1	Yes		Yes	Good
No Crossing Barrier	Route 146	1	Yes		No	
No Crossing Barrier	Route 146 Trail	1	Yes		No	
No Crossing Barrier	Vischer Ferry Rd	1	Yes		Yes	Good
No Crossing Barrier	Vischer Ferry Rd	1	Yes		No	
No Crossing Barrier	Route 146	1	Yes		No	
No Crossing Barrier	Clifton Park Blvd	1	Yes		No	
No Crossing Barrier	Route 146	1	Yes		No	
No Crossing Barrier	Route 146	1	Yes		No	
No Crossing Barrier	Clifton Common Blvd	1	Yes		Yes	Good
No Crossing Barrier	Gloucester St	1	Yes		Yes	Fair
No Crossing Barrier	Gloucester St	1	Yes		Yes	Fair
No Crossing Barrier	Clifton Park Center Rd	1	Yes		Yes	Fair
No Crossing Barrier	Clifton Park Center Rd	1	Yes		Yes	Poor
No Crossing Barrier	Clifton Park Center Rd	1	Yes		No	

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
No Crossing Barrier	Clifton Park Center Rd	1	Yes		Yes	Fair
No Crossing Barrier	Route 9	1	Yes		No	
No Crossing Barrier	Moe Rd	2	Yes		Yes	Good
No Crossing Barrier	Moe Rd	2	Yes		Yes	Good
No Crossing Barrier	Moe Rd	2	Yes		Yes	Good
No Crossing Barrier	Municipal Plz	2	Yes		No	
No Crossing Barrier	Park Ave	2	Yes		Yes	Good
No Crossing Barrier	Clifton Country Rd Extension	2	Yes		No	
No Crossing Barrier	Clifton Country Rd	2	Yes		Yes	Good
No Crossing Barrier	Clifton Country Rd	2	Yes		Yes	Good
No Crossing Barrier	Clifton Country Rd	2	Yes		Yes	Good
No Crossing Barrier	Clifton Country Rd	2	Yes		Yes	Good
No Crossing Barrier	Clifton Country Rd	2	Yes		Yes	Poor
No Crossing Barrier	Clifton Country Rd	2	Yes		Yes	Good
No Crossing Barrier	Wall St	2	Yes		Yes	Poor
No Crossing Barrier	Wall St	2	Yes		No	

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
No Crossing Barrier	Clifton Country Rd	2	Yes		No	
No Crossing Barrier	Clifton Country Rd	2	Yes		No	
No Crossing Barrier	Wall St	2	Yes		No	
No Crossing Barrier	Clifton Country Rd	2	Yes		No	
No Crossing Barrier	Clifton Country Rd	2	Yes		Yes	Good
No Crossing Barrier	Clifton Country Rd	2	Yes		Yes	Good
No Crossing Barrier	Clifton Country Rd	2	Yes		Yes	Good
No Crossing Barrier	Clifton Country Rd	2	Yes		Yes	Good
No Crossing Barrier	Clifton Country Rd	2	Yes		No	
No Crossing Barrier	Crossing Blvd	2	Yes		Yes	Good
No Crossing Barrier	Crossing Blvd	2	Yes		Yes	Good
No Crossing Barrier	Mooney Way	2	Yes		No	
No Crossing Barrier	Abele Blvd	2	Yes		No	
No Crossing Barrier	Moe Rd	2	Yes		Yes	Fair
No Crossing Barrier	Moe Rd	2	Yes		Yes	Good
No Crossing Barrier	Moe Rd	2	Yes		Yes	Good

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
No Crossing Barrier	I87 NB Off Ramp	2	Yes		Yes	Good
No Crossing Barrier	I87 NB Off Ramp	2	Yes		Yes	Good
No Crossing Barrier	Clifton Park Blvd	2	Yes		Yes	Good
No Crossing Barrier	I87 SB On Ramp	2	Yes		Yes	Good
No Crossing Barrier	Walnut Dr	3	Yes		No	
No Crossing Barrier	Crescent Rd	3	Yes		Yes	Good
No Crossing Barrier	Clifton Park Center Rd	3	Yes		Yes	Good
No Crossing Barrier	Arongen Dr	3	Yes		Yes	Good
No Crossing Barrier	Moe Rd	3	Yes		Yes	Good
No Crossing Barrier	School Dr	3	Yes		Yes	Good
No Crossing Barrier	School Dr	3	Yes		Yes	Good
No Crossing Barrier	Vista Ct	4	Yes		No	
No Crossing Barrier	London Square Dr	4	Yes		Yes	Good
No Crossing Barrier	Ushers Rd	4	Yes		Yes	Fair
No Crossing Barrier	Ushers Rd	4	Yes		Yes	Poor
No Crossing Barrier	Ashley Dr	4	Yes		No	

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
No Crossing Barrier	Crown Pt	4	Yes		Yes	Good
No Crossing Barrier	Crown Pt	4	Yes		No	
No Crossing Barrier	Hilltop Hollow Dr	4	Yes		No	
No Crossing Barrier	Chandler Dr	4	Yes		No	
No Crossing Barrier	Ushers Rd	4	Yes		Yes	Poor
No Crossing Barrier	Ushers Rd	4	Yes		Yes	Poor
No Crossing Barrier	Meridian Ln	4	Yes		Yes	Good
No Crossing Barrier	English Rd	4	Yes		Yes	Good
No Crossing Barrier	Nottingham Way N	4	Yes		No	
No Crossing Barrier	Gloucester St	4	Yes		Yes	Good
No Crossing Barrier	Gloucester St	4	Yes		Yes	Good
No Crossing Barrier	Clifton Park Center Rd	4	Yes		Yes	Good
No Crossing Barrier	Blossom Hill Ct	4	Yes		No	
No Crossing Barrier	Blossom Hill Ct	4	Yes		Yes	Good
No Crossing Barrier	Summerlin Dr	4	Yes		No	
No Crossing Barrier	Danbury Ct	4	Yes		No	

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
No Crossing Barrier	Avenue Of The Oaks	4	Yes		No	
No Crossing Barrier	Avenue Of The Oaks	4	Yes		No	
No Crossing Barrier	Leaf Mnr	4	Yes		No	
No Crossing Barrier	Grooms Rd	4	Yes		No	
No Crossing Barrier	Moe Rd	4	Yes		No	
No Crossing Barrier	Balsam Way	4	Yes		Yes	Good
No Crossing Barrier	Balsam Way	4	Yes		No	
No Crossing Barrier	Balsam Way	4	Yes		No	
No Crossing Barrier	Arthur Ct	4	Yes		No	
No Crossing Barrier	Balsam Way	4	Yes		No	
No Crossing Barrier	Van Vranken Rd	4	Yes		No	
No Crossing Barrier	Ashford Dr	4	Yes		No	
No Crossing Barrier	Bridlewood Loop	4	Yes		No	
No Crossing Barrier	Moe Rd	4	Yes		Yes	Good
No Crossing Barrier	Meghan Way	4	Yes		Yes	Good
No Crossing Barrier	Boyack Rd	4	Yes		No	

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
No Crossing Barrier	Oakwood Blvd	4	Yes		Yes	Good
No Crossing Barrier	Carrington Ct	4	Yes		No	
No Crossing Barrier	Easton Dr	4	Yes		No	
No Crossing Barrier	Crescent Rd Trail	4	Yes		No	
No Crossing Barrier	Lapp Rd	4	Yes		Yes	Good
No Crossing Barrier	Lapp Rd	4	Yes		Yes	Good
No Crossing Barrier	Riverview Rd	4	Yes		Yes	Good
No Crossing Barrier	Riverview Rd	4	Yes		Yes	Good
No Crossing Barrier	Riverview Rd	4	Yes		Yes	Good
No Crossing Barrier	Riverview Rd	4	Yes		Yes	Good
No Crossing Barrier	Main St	4	Yes		Yes	Good
No Crossing Barrier	Hollister Ave	4	Yes		Yes	Good
No Crossing Barrier	Center Ct	4	Yes		No	
No Crossing Barrier	Clifton Park Center Rd	4	Yes		Yes	Good
No Crossing Barrier	Katherine Ter	4	Yes		Yes	Good
No Crossing Barrier	Kingswood Dr	4	Yes		Yes	Poor

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
No Crossing Barrier	Moe Rd	4	Yes		Yes	Good
No Crossing Barrier	Moe Rd	4	Yes		Yes	Good
No Crossing Barrier	Stratford Dr	4	Yes		Yes	Good
No Crossing Barrier	Stratford Dr	4	Yes		Yes	Good
No Crossing Barrier	Dawson Ln	4	Yes		Yes	Good
No Crossing Barrier	Moe Rd	4	Yes		Yes	Good
No Crossing Barrier	Acorn Ave	4	Yes		Yes	Good
No Crossing Barrier	Route 146	4	Yes		No	
No Crossing Barrier	Route 146	4	Yes		No	
No Crossing Barrier	Route 146	4	Yes		No	
No Crossing Barrier	Blueberry Ln	4	Yes		Yes	Good
No Crossing Barrier	Honey Hollow Way	4	Yes		Yes	Good
No Crossing Barrier	Denim Dr	4	Yes		Yes	Good
No Crossing Barrier	Lace Ln	4	Yes		Yes	Good
No Crossing Barrier	Vischer Ferry Rd	4	Yes		Yes	Good
No Crossing Barrier	Vischer Ferry Rd	4	Yes		Yes	Good

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
No Crossing Barrier	Vischer Ferry Rd	4	Yes		No	
No Crossing Barrier	Vischer Ferry Rd	4	Yes		No	
No Crossing Barrier	Leaf Mnr	4	Yes		Yes	Good
No Crossing Barrier	Moe Rd	4	Yes		Yes	Poor
No Crossing Barrier	Route 9	5	Yes		Yes	Fair
No Crossing Barrier	I87 NB On Ramp	5	Yes		Yes	Good
No Crossing Barrier	I87 NB On Ramp	5	Yes		Yes	Good
No Crossing Barrier	I87 SB Off Ramp	5	Yes		Yes	Good
No Crossing Barrier	Van Patten Dr	5	Yes		Yes	Good
No Crossing Barrier	Fairchild Sq	5	Yes		Yes	Good
No Crossing Barrier	Fairchild Sq	5	Yes		Yes	Good
No Crossing Barrier	Fairchild Sq	5	Yes		Yes	Good
No Crossing Barrier	Factron Sq	5	Yes		Yes	Good
No Crossing Barrier	Factron Sq	5	Yes		Yes	Good
No Crossing Barrier	Crescent Rd Trail	5	Yes		No	
No Crossing Barrier	John J McKenna Iv Way	5	Yes		No	

Accessibility Rating	Road Name	Priority	Pedestrian Access Route?	Deterioration Type	Designated Crosswalk?	Crosswalk Condition
No Crossing Barrier	John J McKenna Iv Way	5	Yes		No	
No Crossing Barrier	John J McKenna Iv Way	5	Yes		No	
No Crossing Barrier	John J McKenna Iv Way	5	Yes		Yes	Good
No Crossing Barrier	John J McKenna Iv Way	5	Yes		Yes	Good
No Crossing Barrier	Crescent Rd	5	Yes		No	
No Crossing Barrier	Crescent Rd	5	Yes		No	
No Crossing Barrier	Crescent Rd	5	Yes		No	
No Crossing Barrier	Crescent Rd Trail	5	Yes		No	
No Crossing Barrier	Route 146A	5	Yes		Yes	Good
No Crossing Barrier	146A Ramp	5	Yes		Yes	Good
No Crossing Barrier	Clifton Park Blvd	5	Yes		Yes	Good

# APPENDIX E: ADA POLICY STATEMENT

## NOTICE UNDER THE AMERICANS WITH DISABILITIES ACT

In accordance with the requirements of title II of the Americans with Disabilities Act of 1990 ("ADA"), the Town of Clifton Park will not discriminate against qualified individuals with disabilities on the basis of disability in its services, programs, or activities.

**Employment:** The Town of Clifton Park does not discriminate on the basis of disability in its hiring or employment practices and complies with all regulations promulgated by the U.S. Equal Employment Opportunity Commission under title I of the ADA.

**Effective Communication:** The Town of Clifton Park will generally, upon request, provide appropriate aids and services leading to effective communication for qualified persons with disabilities so they can participate equally in the Town of Clifton Park's programs, services, and activities, including qualified sign language interpreters, documents in Braille, and other ways of making information and communications accessible to people who have speech, hearing, or vision impairments.

**Modifications to Policies and Procedures:** The Town of Clifton Park will make all reasonable modifications to policies and programs to ensure that people with disabilities have an equal opportunity to enjoy all of its programs, services, and activities. For example, individuals with service animals are welcomed in the Town of Clifton Park offices, even where pets are generally prohibited.

Anyone who requires an auxiliary aid or service for effective communication, or a modification of policies or procedures to participate in a program, service, or activity of the Town of Clifton Park, should contact the ADA Compliance Officer at 1 Town Hall Plaza Clifton Park, NY 12065 Telephone: (518) 371-6651, as soon as possible but preferably no later than 48 hours before the scheduled event. Requests later than 48 hours will be accommodated to the extent possible.

The ADA does not require the Town of Clifton Park to take any action that would fundamentally alter the nature of its programs or services or impose an undue financial or administrative burden.

Complaints that a program, service, or activity of the Town of Clifton Park is not accessible to persons with disabilities should be directed to the ADA Compliance

Officer 1 Town Hall Plaza  
Clifton Park, NY 12065, (518) 371-6651.

The Town of Clifton Park will not place a surcharge on a particular individual with a disability or any group of individuals with disabilities to cover the cost of providing auxiliary

aids/services or reasonable modifications of policy, such as retrieving items from locations that are open to the public but are not accessible to persons who use wheelchairs.

# APPENDIX F: GRIEVANCE PROCEDURE

## Town of Clifton Park Grievance Procedure Under the Americans with Disabilities Act

This Grievance Procedure is established to meet the requirements of the Americans with Disabilities Act of 1990 ("ADA"). It may be used by anyone who wishes to file a complaint alleging discrimination on the basis of disability in the provision of services, activities, programs, or benefits by the Town of Clifton Park. The Town of Clifton Park's Personnel Policy governs employment-related complaints of disability discrimination.

The complaint should be in writing and contain information about the alleged discrimination such as name, address, phone number of complainant and location, date, and description of the problem. Alternative means of filing complaints, such as personal interviews or a tape recording of the complaint, will be made available for persons with disabilities upon request.

The complaint should be submitted by the grievant and/or their designee as soon as possible but no later than 60 calendar days after the alleged violation to:

1 Town Hall Plaza  
Clifton Park, NY 12065,  
(518) 371-6651

Within 60 days after the filing of the complaint, an investigation shall be conducted and completed, including, if necessary, a meeting with the complainant to discuss the complaint and possible resolutions. No later than 60 days after complaint submittal, a determination shall be issued by the ADA Coordinator in writing, and where appropriate, in a format accessible to the complainant, such as large print, Braille, or audio tape. The determination will explain the position of the Town of Clifton Park and offer options for substantive resolution of the complaint.

If the determination by the ADA Coordinator in writing does not satisfactorily resolve the issue, the complainant and/or his/her/their designee may appeal the decision within 30 calendar days after receipt of the response to the ADA Coordinator.

The Town's ADA Coordinator, in writing will review the complaint and, if necessary, meet with the complainant to discuss the complaint and possible resolutions. Within 60 calendar days after receipt of the appeal, the ADA Coordinator, will respond in writing, and, where appropriate, in a format accessible to the complainant, with a final resolution of the complaint.

All written complaints received by the ADA Coordinator, in writing, and responses will be retained by the Town of Clifton Park for at least three (3) years.

# APPENDIX G: ADA COORDINATOR CONTACT INFORMATION

Town of Clifton Park ADA Coordinator

All complaints and/or questions regarding accessibility of any public buildings, pedestrian facilities within the right-of-way, or other programs, services, or activities of the Town of Clifton Park should be directed to designated official below.

**Title:**

**Telephone:**

**Mailing Address:**

One Town Hall Plaza

Clifton Park, NY

12065

# APPENDIX H: PUBLIC COMMENT